



**VILLAGE OF
GLENVIEW**
DOWNTOWN DEVELOPMENT CODE



Sec. 98-135. D-D downtown development district regulations.

(a) *Introduction.*

(1) *Code Components.*

- Subsection (b): General provisions: This section describes the purpose, intent, and applicability of the Downtown Development Code in the Village of Glenview.
- Subsection (c): Regulating framework: This section defines the community's vision for downtown as determined by the village's extensive public planning process. It provides regulations on land uses, building heights, and parking based on the Downtown Revitalization Plan and 2021 Downtown Strategic Plan's recommendations.
- Subsection (d): Street frontages: This section categorizes downtown streets according to the desired physical character recommended by the Downtown Plan and 2021 Downtown Strategic Plan's recommendations. It addresses optimal building setbacks, step-backs, and types; site access; and parking placement.
- Subsection (e): Design standards: This section contains standards that define building, streetscape, landscape, and site design.
- Subsection (f): Administration: This section presents the application and review process for downtown development.
- Subsection (g): Definitions: This section is a glossary of technical terms. Except where terms are undefined, the definitions in this section shall supersede the definitions of section 98-4 of this Code.
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(b) *General provisions.*

- (1) *Title.* This section shall be known as the "Downtown Development Code" for the Village of Glenview or the "Code."
- (2) *Applicability.* The regulations of this code shall apply to all development, public or private, within the Downtown Development District, or "district," as designated on the attached zoning map, unless otherwise expressly exempted or provided in this Downtown Development Code (see areas shaded in gray in Figure b-1 and Figure d-1).

The district generally encompasses Glenview Road from Washington Street to Waukegan Road, Waukegan Road from Lake Avenue to Henley Street, Railroad Avenue from Dewes Street to just north of Grove Street, Dewes Street from Washington to Waukegan, and portions of Pine, Grove, Church, Prairie, Harlem, McLean, and Maplewood that are side streets for the major roads in Downtown.

- (3) *Effective date.* This section was adopted via Ordinance No. 6466 and became effective on January 18, 2022.

Figure b-1: Downtown Development Code Street Frontages



- (4) *Purpose and authority.* This code is adopted pursuant to the Home Rule powers of the Village of Glenview under the 1970 constitution of the State of Illinois. This code is adopted for the purpose of:
- a. Promoting the public health, safety, and general welfare;
 - b. Improving the village's quality of life for residents and visitors by enhancing downtown's "sense of place;"
 - c. Implementing the policies and goals contained within the Village's Downtown Revitalization Plan and 2021 Downtown Strategic Plan;
 - d. Maintaining orderly and compatible land-use and development patterns;
 - e. Enhancing and expanding downtown's retail environment by clustering shopping opportunities;

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- f. Expanding residential opportunities, both to improve housing choice and increase foot traffic and vitality;
 - g. Promoting rehabilitation and reuse of older buildings;
 - h. Accommodating office uses in the right locations;
 - i. Ensuring adequate light, air, privacy, and access to property;
 - j. Providing for adequate storm water drainage;
 - k. Enhancing pedestrian access and circulation;
 - l. Providing efficient parking and transportation facilities; and
 - m. Accommodating growth and development that complies with the preceding stated purposes.
- (5) *Intent.*
- a. The intent of this Downtown Development Code is to implement the Downtown Revitalization Plan adopted by the Village Board of Trustees on August 15, 2006 and the 2021 Downtown Strategic Plan adopted by the Village Board of Trustees on August 3, 2021.
 - b. This section focuses on the creation of mixed-use, walkable neighborhoods, and emphasizes building "form," "public realm," and high-quality site and building design. It is intended to set the foundation for a vital, attractive downtown by encouraging streets, blocks, and buildings that are pedestrian-oriented, have a mix of uses, contain architectural diversity, and offer additional opportunities to live, work, and shop in the village. Downtown Plan implementation is intended to:
 - 1. Create a sense of place that has uses desired and needed by the community;
 - 2. Enhance the aesthetic appeal of the downtown;
 - 3. Provide desirable amenities such as parking, open space and streetscape improvements; and
 - 4. Address the needs of the downtown merchants, while attracting additional high-quality establishments.
- More specific statements of intent are found within each major section of this Downtown Development Code.
- (6) *Adopted plans.* The following plans have been adopted by the village board of trustees and should be considered in any decisions under this Downtown Development Code in the D-D Downtown Development District:
- a. Downtown Strategic Plan (2021).
 - b. Comprehensive Plan (2017).
 - c. Downtown Revitalization Plan (2006).
- (7) *Minimum requirements.* The provisions of this Downtown Development Code are the minimum requirements deemed necessary to carry out its stated purposes and intent.
- (8) *Conflicting provisions.* Conflict with private agreements and covenants:
- This Downtown Development Code is not intended to interfere with, abrogate or annul any easement, covenant, deed restriction or other agreement between private parties. The village does not enforce or maintain a record of private agreements.

(9) *Severability.* Should any section, paragraph, sentence, clause, phrase, or word in this Downtown Development Code be declared invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect any of the remaining words, phrases, clauses, sentences, paragraphs or sections of this Downtown Development Code, since they would have been enacted by the president and board of trustees without the incorporation in this Downtown Development Code of any such invalid or unconstitutional word, phrase, clause, sentence, paragraph, or section.

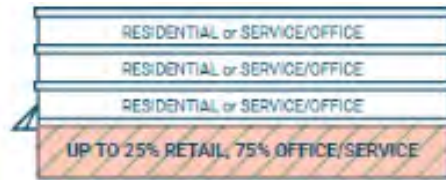
(c) *Regulating framework.*

(1) *Purpose.* The regulating framework defines the desired physical form for Downtown Glenview and sets development parameters such as land use, street type, building height, siting and setbacks, and parking placement. The Development Code incorporates the recommendations of the Downtown Strategic Plan and Downtown Revitalization Plan but also applies to, and regulates, parcels not specifically referenced in these plans.

(2) *Permitted uses.* The following is a general description of land uses allowed in the D-D Downtown Development District (see Figure c-1 for the location of each use by block). For a list of specific permitted and conditional uses, see section 98-84.



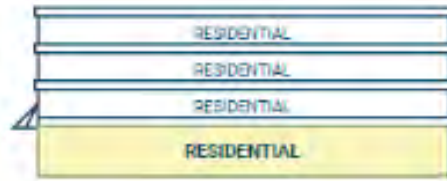
a. *Full ground-floor retail.* These buildings are required to have retail or service uses on the ground floor. At least 75 percent of the building's gross ground floor area within the frontage (as measured 30 feet from the building line) must be retail, while service uses can occupy the remaining 25 percent. Service and office uses are only allowed on upper floors along these blocks. Upper-floor residential uses also are allowed.



b. *Partial to full ground-floor retail.* Retail, service or office uses are required on the ground floor along these blocks. At least 25 percent of the building's gross ground floor area within the frontage (as measured 30 feet from the building line) must be retail, while service and office uses can occupy the remaining 75 percent. Upper-floor residential and service/ office uses also are allowed.



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- c. *Ground-floor retail/office/service.* Retail, service, or office uses are encouraged if feasible on the ground floor along blocks with this designation. Residential, service, and office uses are allowed on upper floors. All-residential buildings also are allowed.



- d. *Residential uses only.* Only residential uses are allowed along blocks with this designation.



- e. *Retail/office/service uses only.* Only retail, office, or services are allowed.



- f. *Institutional uses.* Civic or institutional uses such as libraries, churches, and schools are allowed.

Figure c-1: Downtown Uses



- g. *Below-ground uses.* Ancillary commercial, ancillary residential, parking, storage, and mechanical uses are allowed in a basement or below grade floors. If commercial uses are permitted on the ground floor, such uses are also permitted in the basement level as long as access to commercial space is separate from access to upper story residential units.
 - h. *Depths of use.* Depths of use requirements shown in Figure c-1 extend 30 feet from the Building Line.
 - i. *Transition of use around corners.* Street Frontages along Glenview and Waukegan Road shall transition around the first 30 feet of the street frontage of the intersecting street.
- (3) *Minimum commercial depth.* Commercial uses must have a minimum depth (from the front building line to the rear of the retail floor area) of 30 feet.

(4) *Uses on the same floor.*

- a. While uses can be "mixed" within buildings by floor as noted above, residential and commercial (retail, service, or office) uses cannot be mixed on the same floor in a building above the ground floor. Residential can be mixed with commercial uses on the ground floor but not on the same street frontage. Residential lobbies, leasing office, amenity space, or building entrances are exempt from this standard.
- b. When transitioning between commercial and residential uses around corners, building elements such as secondary entrances, plazas/paseos/walkways, parking access, or other features shall be used to separate uses.
- c. A residential lobby/elevator is required on the ground floor in all buildings with residential uses on upper floors. Commercial uses on the first floor may have a secondary entrance from this lobby as long as their primary commercial entrance is from the sidewalk on the street.
- d. Parking is allowed on the ground floor behind commercial uses in buildings with retail, service, or office uses on the ground floor.
- e. In addition to the use information noted above by location, see subsection (d), Street frontages, for more specific regulations on allowed uses.

(5) *Building heights.* Figure c-2 depicts the maximum number of stories allowed in specific areas of the D-D Downtown Development District.

Table c-1, Table c-2, and Table c-3 provide the maximum allowable height in stories and feet. All dimensions are measured from top of finished floor to top of finished floor/roof deck. The zero-foot elevation is measured on a horizontal plane at the average of the four existing grades (prior to commencement of demolition/construction) measured at the four corners of the proposed building or structure located nearest the most distant lot corners.

- a. Floor heights by story for multi-story commercial or mixed-use buildings: The ground floor shall be at least 15 and no more than 17 feet tall, with the upper residential or office floors being at least ten and no more than 12 feet tall.
- b. Floor heights by story for multi-family residential buildings: All floors shall be at least ten and no more than 12 feet tall.
- c. Floor heights for Waukegan Road north of Grove Street and 155 feet south of Lake Avenue ROW (up to existing corner lot) on the west side of Waukegan: For mixed-use buildings, the ground floor shall be 15 feet, with upper residential floors being at least ten and no more than 11 feet tall.
- d. All building heights are inclusive of pitched roofs, building parapets, and mechanical screening. Buildings are encouraged to include varied roof heights and forms and discouraged from large areas of flat roof planes.
- e. Elevator penthouses are exempt from the building height requirements listed in Table c-1, Table c-2, and Table c-3 provided they are located at least 30 feet behind the primary building facade and serve accessible roof decks or terraces. In these cases, elevator penthouses can extend up to 15 feet above the top of the roof deck provided the maximum area of such penthouse(s) cannot exceed 250 square feet per elevator.
- f. Decks/terraces: Use of terraces created by building step-backs are permitted and encouraged. Decks or terraces are not permitted on rooftops, except within 50 feet of the Glenview Road street frontage.

Figure c-2: Maximum Building Heights

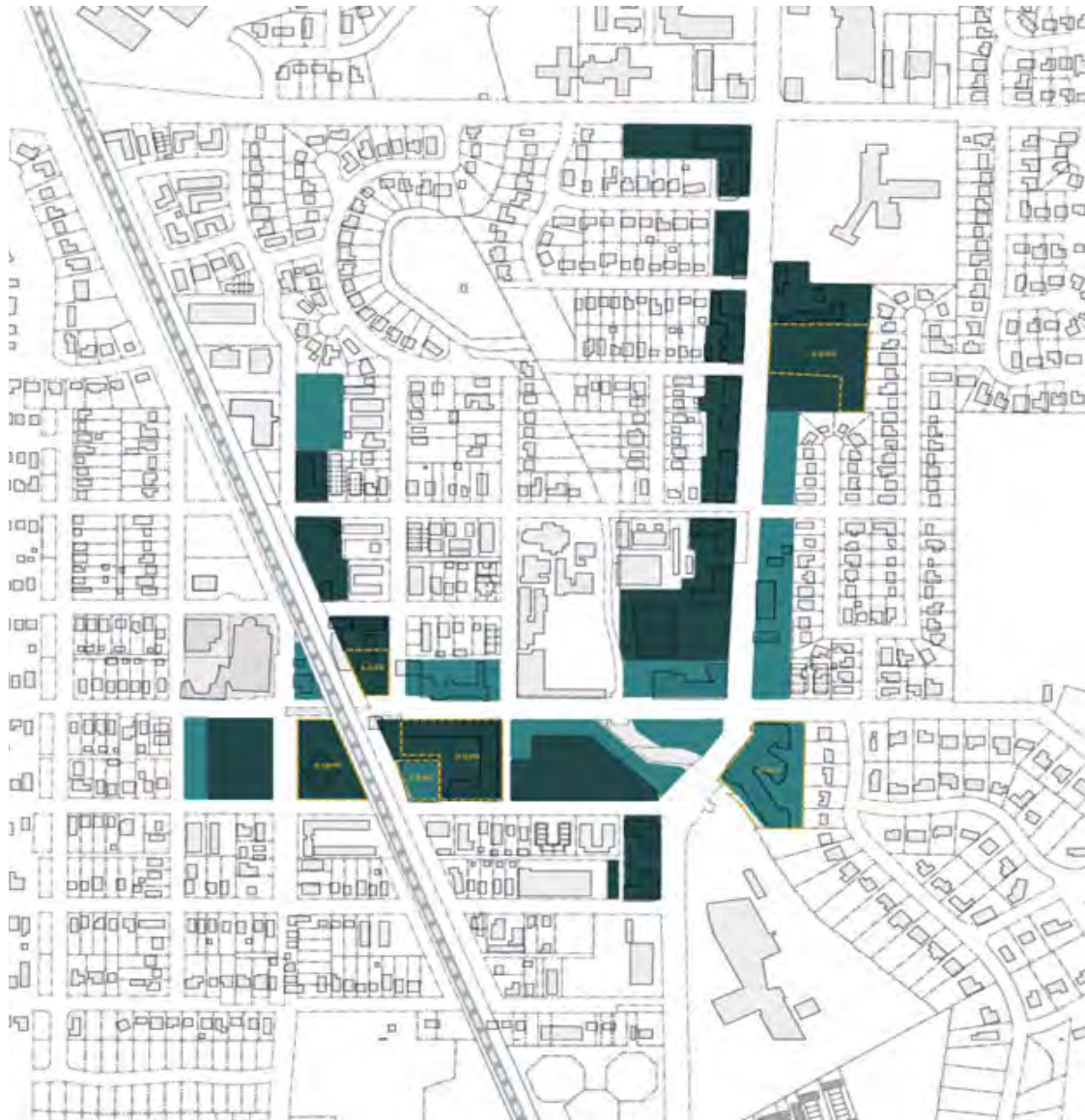


Table c-1: Maximum Height—Commercial/Residential Mixed-Use

COMMERCIAL/RESIDENTIAL MIXED-USE HEIGHTS			
Number of Stories	MAXIMUM FLOOR HEIGHT	TOP OF ROOF DECK	TOTAL MAXIMUM HEIGHT ROOFS, EQUIPMENT, PARAPETS, ARCHITECTURAL FEATURES
4*	12	53	63
3	12	41	51
2	12	29	39
1	17	17	22

Table c-2: Maximum Height—Multi-family Residential

MULTI-FAMILY HEIGHTS			
Number of Stories	MAXIMUM FLOOR HEIGHT	TOP OF ROOF DECK	TOTAL MAXIMUM HEIGHT
			ROOFS, EQUIPMENT, PARAPETS, ARCHITECTURAL FEATURES
4*	12	48	58
3	12	36	46
2	12	24	34

Table c-3: Maximum Height—Commercial/Residential Mixed-Use, Waukegan Rd.

COMMERCIAL/RESIDENTIAL MIXED-USE HEIGHTS WAUKEGAN ROAD, NORTH OF GROVE ST., SOUTH OF EAST LAKE AVE.			
Number of Stories	MAXIMUM FLOOR HEIGHT	TOP OF ROOF DECK	TOTAL MAXIMUM HEIGHT
			ROOFS, EQUIPMENT, PARAPETS, ARCHITECTURAL FEATURES
4*	11	48	58
3	11	37	47
2	11	26	36
1	15	15	20

* Only if allowed in accordance with the quantity of stories permissible in accordance with Figure c-2.

- (6) *Parking.* A minimum quantity of off-street parking stalls shall be provided for each use according to the following ratios:
- a. *Minimum parking by use.*
 1. *Townhomes/rowhomes.* Two spaces per unit.
 2. *Multi-family.* One space per bedroom for one- and two-bedroom units, 0.75 spaces per bedroom for each bedroom over two. Seventy-five percent of all parking spaces shall be located indoors.
 3. *Retail/service.* One space per 300 gross square feet (except in the Glenview Road Corridor Frontage).
 4. *Office.* One space per 300 gross square feet (except in the Glenview Road Corridor Frontage).
 5. *Restaurant.* One space per 160 gross square feet.
 - b. *Exemptions from minimum parking by use.* For any new development project, the first 2,000 square feet of retail, office, or service use shall be exempt from the calculation for required parking spaces. The exemption shall be applied to the least generating use on the ground floor. Only one exemption shall be allowed per building.

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- c. *Minimum parking—Glenview Road street frontage.* For any new development project, the first 2,000 square feet of retail, office, or service use shall be exempt from the calculation for required parking spaces. The exemption shall be applied to the least generating use on the ground floor. Only one exemption shall be allowed per building.
1. Retail, office, and service uses in this frontage are required to provide one space per 500 gross square feet of floor area due to the availability of street parking and commuter parking along the tracks.
 2. Restaurants and residential uses shall provide parking according to the ratios listed in section 98-135(c)(6)a.5.
 3. In the Glenview Road Corridor frontage, when a new use occupies a building existing as of May 21, 1996, additional off- street spaces are not required, provided that parking serving the building was not eliminated or reduced after May 21, 1996.
- d. *Shared parking.*
1. *Collective provisions.* Shared parking should be considered to minimize the visual impact of land devoted to parking and to provide more efficient parking.
 2. *Location.* All required parking spaces shall be on the same lot as the building or use being served or within 500 feet of the property line (see Figure c-4), provided that no off-street parking for a business use shall be in a residential district.
 3. *Mixed-use shared parking.* New buildings or developments can use Table c-4 to calculate shared parking provided they include more than one use listed in the table, and are located within the same block or parcel. To use the table, apply the use percentages to the maximum parking demand required per section 98-135(c)(6)a. and add all of the shared parking totals for each column. The largest number of the four columns is the maximum parking demand and is the required parking for new development. All shared parking plans will require a parking management plan to show maximum parking demand by use, proposed shared parking reductions, number of publicly accessible spaces, and designated/reserved spaces (if applicable).
 4. *Waiver.* The director of community development may waive the number of spaces required in part or entirely if shared parking is deemed sufficient based on the location of the proposed use, anticipated hours of peak parking demand, shared parking, and availability of alternative parking. An executed written agreement covering such collective use shall be filed with the director of community development or their designee.

Figure c-3: Shared Parking

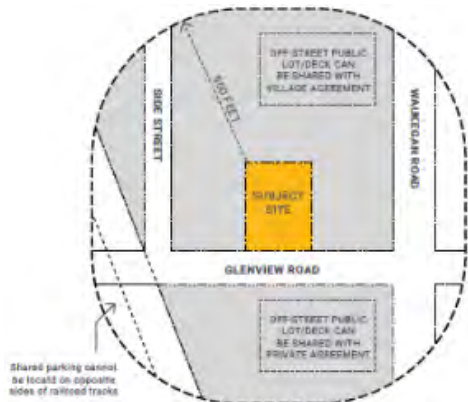


Table c-4: Shared Parking Table

Shared Parking				
	Weekday		Weekend	
	Day	Evening	Day	Evening
Residential	60%	100%	60%	100%
Retail	90%	72%	90%	50%
Restaurant	56%	97%	37%	89%
Office	90%	7%	80%	20%
Hotel	30%	90%	30%	90%
Entertainment	56%	97%	37%	89%

(d) *Street frontages.*

- (1) *Purpose and intent.* Each downtown street has been assigned a street frontage designation to define an optimal "building envelope" and "public realm" using setbacks, sidewalk and street widths, and other design parameters.

Along with subsection (e), Design standards, the required street, sidewalk and building relationships are intended to foster new development that enhances and expands the traditional "Main Street" scale of downtown.

Figure d-1 assigns designations to all street frontages in downtown and establishes a building line for each frontage.

- (2) *Building line.*

- a. *Building line.* The building line is a "build-to" line that will help establish consistent "streetwalls," sidewalk widths, and streetscape zones (see Figure d-1).
- b. *Relationship of building line to right-of-way or property line.* The building line may differ from the current location of a right-of-way or property line. In these cases, because the building line must be "built to," the right-of-way (ROW)/property lines will need to be adjusted through dedication of property between the property owner and village. More specifically, the property owner may

need to dedicate property to the public ROW to create the desirable street and streetscape width, or the village may transfer ROW to the buildable development site.

1. For Glenview Road, the building line is intended to eventually bring all buildings more in line with an 18-foot sidewalk width east of the river and a common 15-foot sidewalk width west of the river.
 2. For Waukegan Road, the building line is intended to eventually eliminate parking lots in front of buildings and bring buildings closer to the sidewalk to better frame the street, reduce its perceived width and scale, and establish this roadway as a physical part of the traditional downtown starting at Lake Avenue. The code also incorporates foundation landscaping in front of all-residential buildings and a landscaped median in the center of Waukegan Road to provide a "green," attractive entrance to the downtown.
 3. For residential buildings along Waukegan Road, the building line provides room for a consistent sidewalk/streetscape space that will accommodate foundation landscaping in front of buildings.
- c. Where the building line designated in this section differs from the Subdivision Code, chapter 66, section 66-165, the provisions of the Downtown Development Code shall apply.
- (3) *Street standards.*
- a. *Purpose and intent.* The street standards defined for each street frontage delineate street and sidewalk conditions that will establish a more attractive, pedestrian-oriented shopping environment.
 1. Over time, desired or common sidewalk widths will be established to create a more regular walking experience from block to block that incorporates a minimum five-foot "free zone" walking area and additional space for street trees, plantings in raised planters, or parkways.
 2. Developers are required to install village-approved streetscape elements for new developments.
 - b. *Required.* All developments within the D-D Downtown Development District must comply with the Code's street standards.

(4) *Street frontages.*

Glenview Road Corridor: Downtown's primary pedestrian-oriented shopping street.

Waukegan Road Corridor: An auto-oriented corridor where the Downtown Revitalization Plan seeks to encourage a more pedestrian-oriented character similar to Glenview Road, especially south of Grove.

Secondary street: Secondary street frontages are also intended to provide a walking shopping/office/service environment while allowing all-residential buildings and respecting adjacent residential uses.

Transition Street: This frontage provides a transition between commercial and residential areas (along Washington and parts of Glenview and Dewes.)

River corridor: This corridor defines regulations to protect the Chicago River, increase its visibility, promote public access to the waterfront, and establish a central open space as delineated in the Downtown Revitalization Plan.

Where street frontage designations change along a street, either designation can be applied for a maximum of 75 feet in either direction along that frontage. Developments on corner lots should use standards for the building's primary street and standards for the "side street" frontage.

Figure d-1: Street Frontages



Figure d-2: Setbacks - River Drive Vacated

DOWNTOWN GLENVIEW SETBACKS IF RIVER DRIVE IS VACATED

See subsection [\(d\)\(10\)](#).



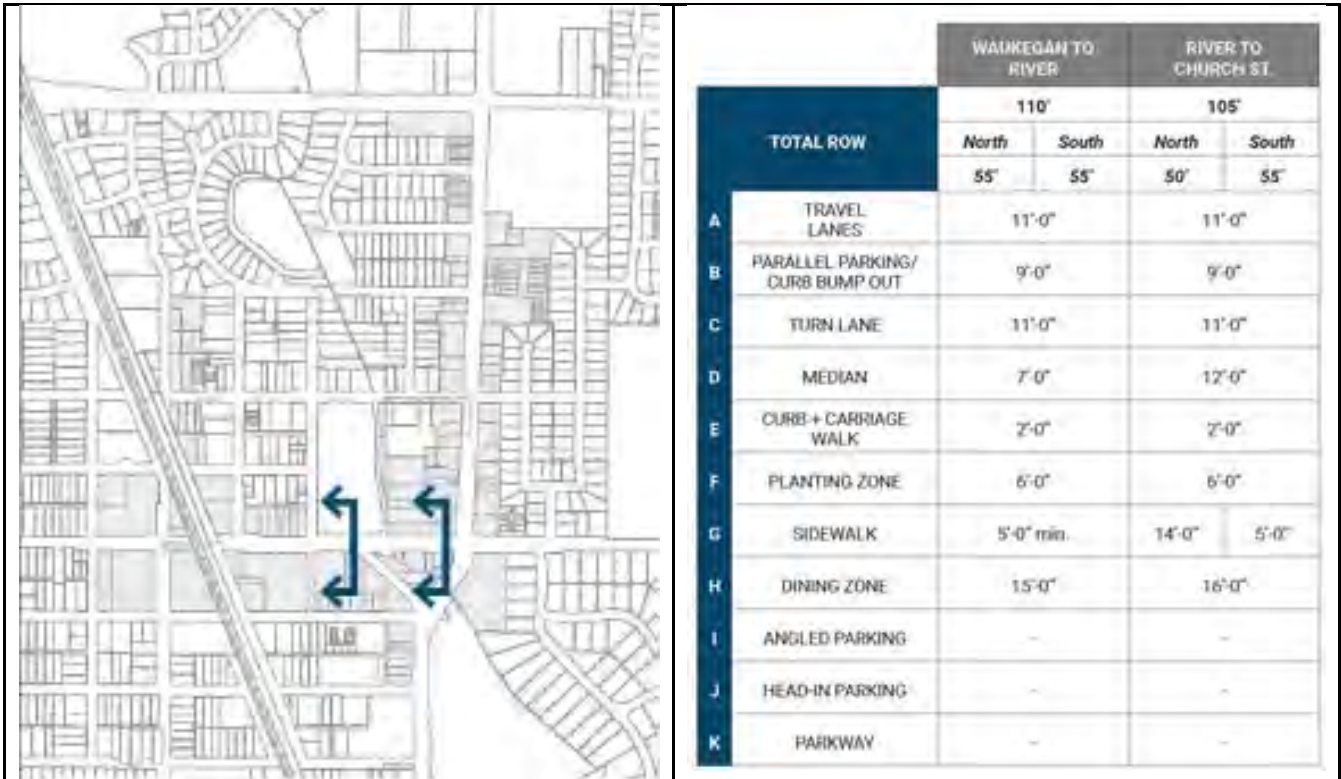
Figure d-3: Setbacks



- (5) *Street setbacks.* See subsections (d)(6) through (d)(9) for more specific details on the dimensions of setbacks within each frontage zone.
- (6) *Glenview Road Corridor.*
 - a. *Purpose and intent.* The Glenview Road Corridor is applied to Downtown Glenview's traditional shopping street. This stretch of Glenview Road has a pedestrian-oriented, "Main Street" character, and the Downtown Revitalization Plan and Development Code seek to enhance this character by providing a consistent building "streetwall," and a streetscape/sidewalk zone that allows ample room for walking, shopping, outdoor dining, and landscaping.
 - b. *Street/sidewalk standards.*

Glenview Rd. ROW Segments	TOTAL ROW	Figure
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Waukegan Rd. to River	110'	Figure d-4
River to Church St.	105'	Figure d-5
Church St. to Pine St.	97'	Figure d-6
Pine St. To Railroad ROW	88.5'	Figure d-7
Railroad ROW to Harlem Ave.	100'	Figure d-8
Harlem Ave. to Washington St.	100'	Figure d-9



These tables represent typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figure d-4: Glenview Road: Waukegan to River

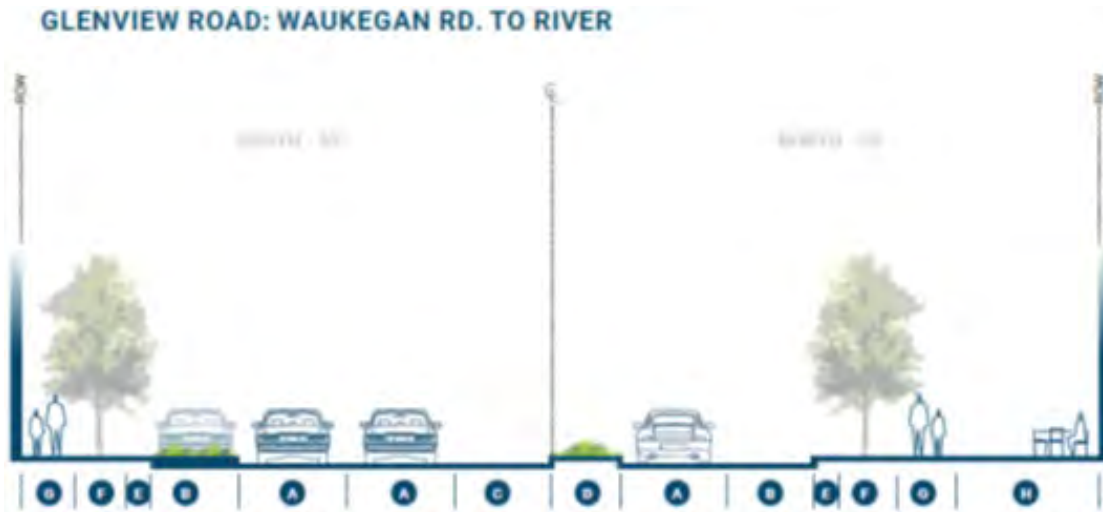
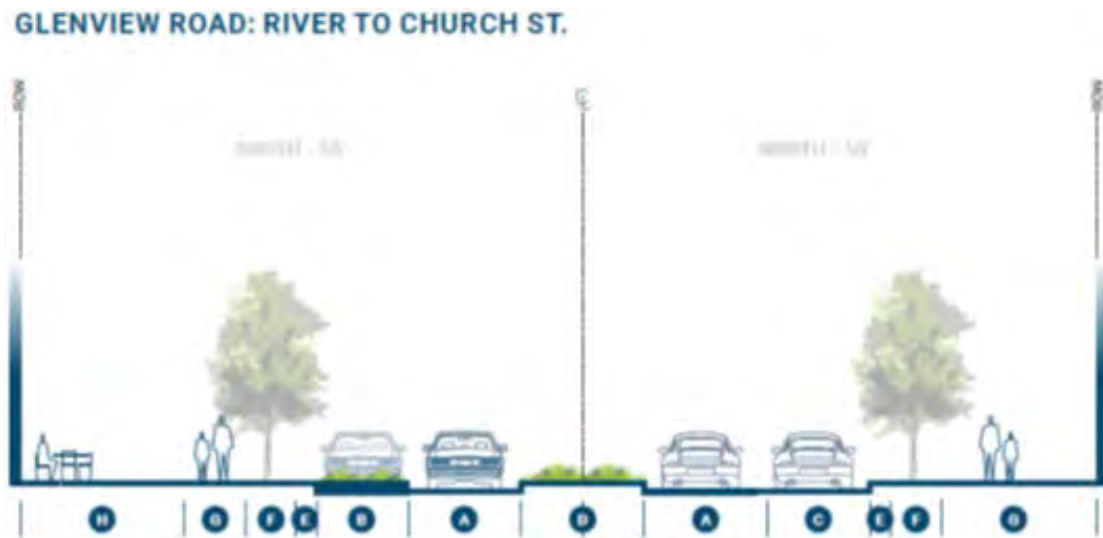
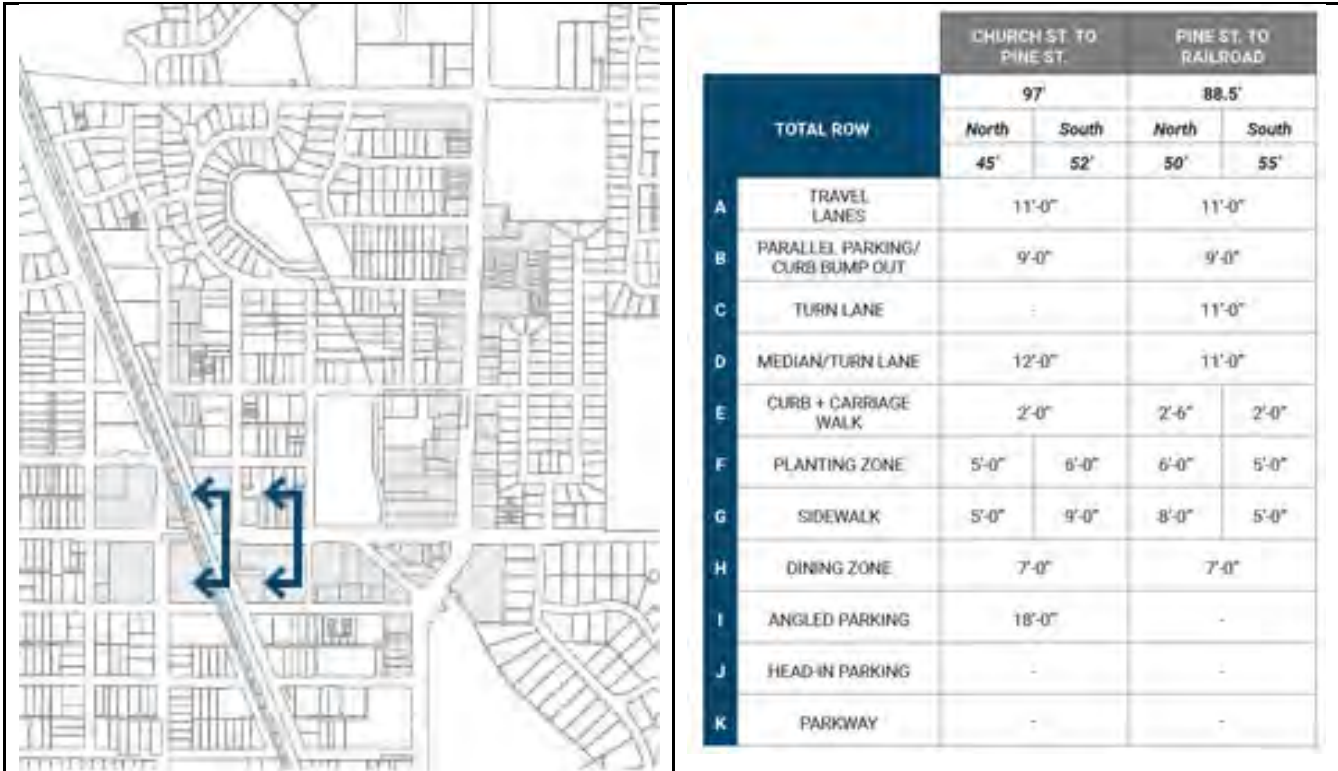


Figure d-5: Glenview Road: River to Church St.





These tables represent typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figures d-6: Glenview Road: Church St. to Pine St.

GLENVIEW ROAD: CHURCH ST. TO PINE ST.

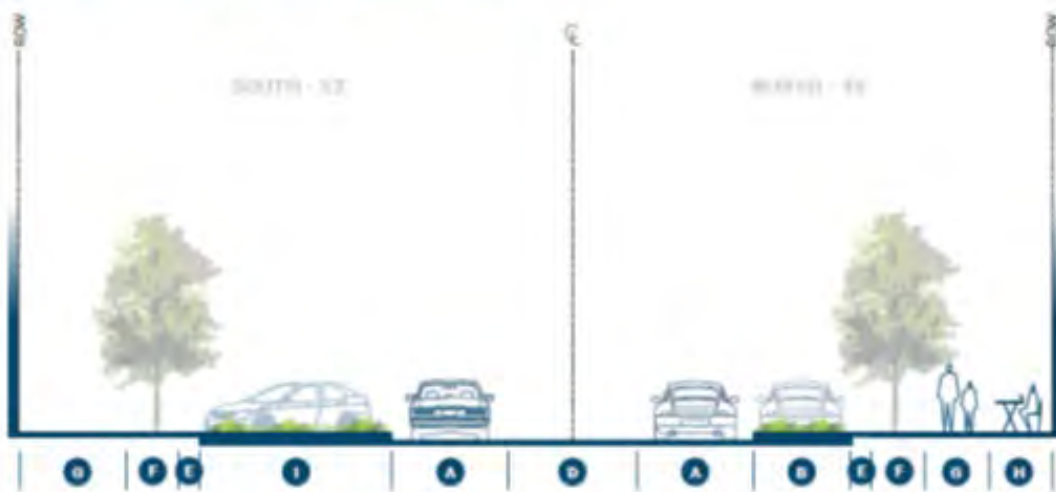
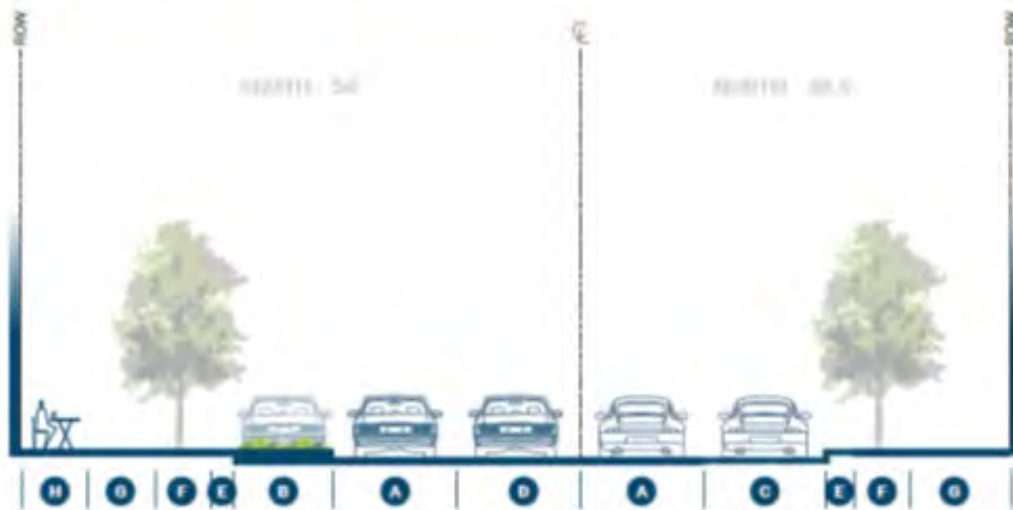


Figure d-7: Glenview Road: Pine St. to Railroad ROW

GLENVIEW ROAD: PINE ST. TO RAILROAD ROW



	RAILROAD TO HARLEM AVE.		HARLEM AVE. TO WASHINGTON ST.	
	TOTAL ROW		TOTAL ROW	
	North	South	North	South
	50'	50'	50'	50'
A	TRAVEL LANES		11'-0"	11'-0"
B	PARALLEL PARKING/ CURB BUMP OUT		-	9'-0"
C	TURN LANE		11'-0"	-
D	MEDIAN/TURN LANE		11'-0"	11'-0"
E	CURB + CARRIAGE WALK		2'-6"	0'-6" 2'-6"
F	PLANTING ZONE		8'-6"	18'-0" 6'-0"
G	SIDEWALK		6'-0" min.	6'-0"
H	DINING ZONE		11'-0"	10'-0"
I	ANGLED PARKING		-	-
J	HEAD-IN PARKING		-	-
K	PARKWAY		-	9'-0"

These tables represent typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figure d-8: Glenview Road: Railroad ROW to Harlem Ave.

GLENVIEW ROAD: RAILROAD ROW TO HARLEM AVE.

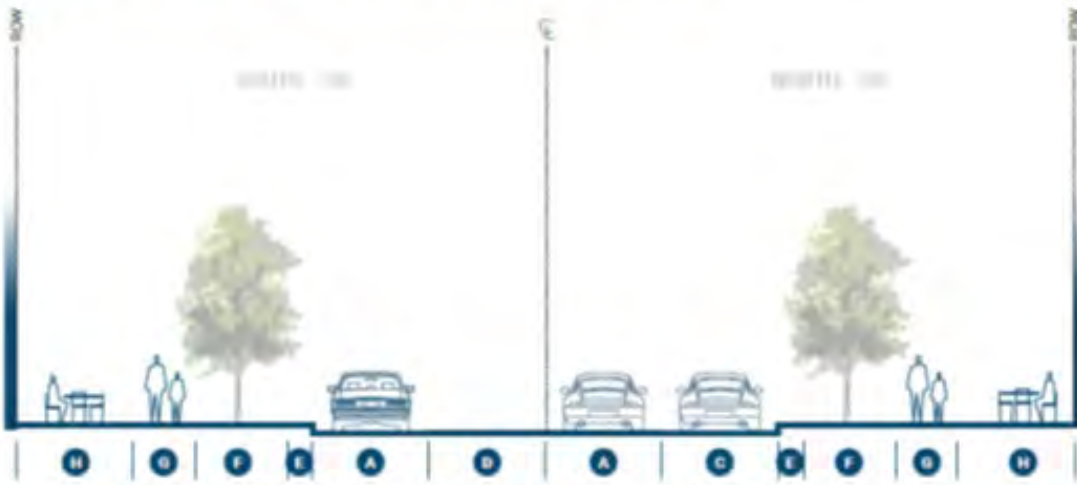
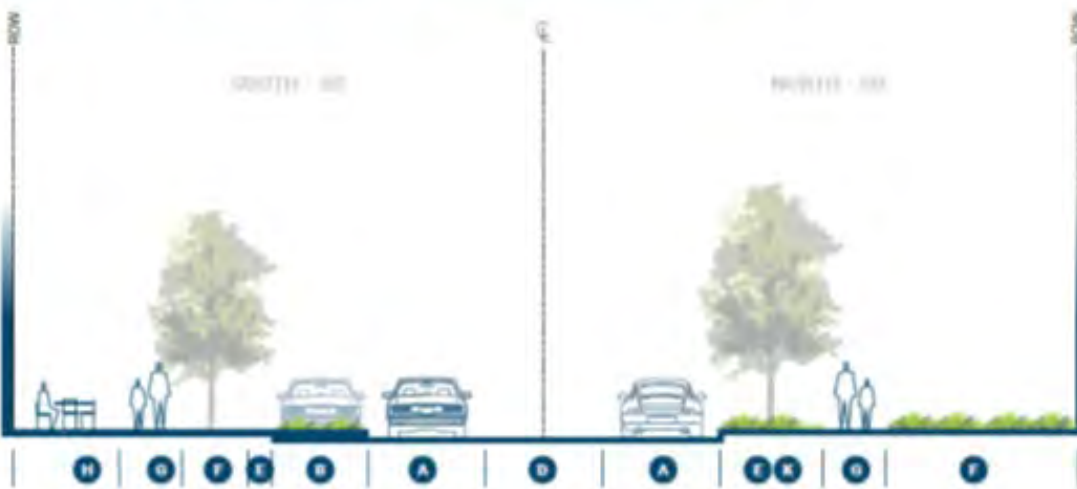


Figure d-9: Glenview Road: Harlem Ave. to Washington St.

GLENVIEW ROAD: HARLEM AVE. TO WASHINGTON ST.



c. *Building setbacks.*

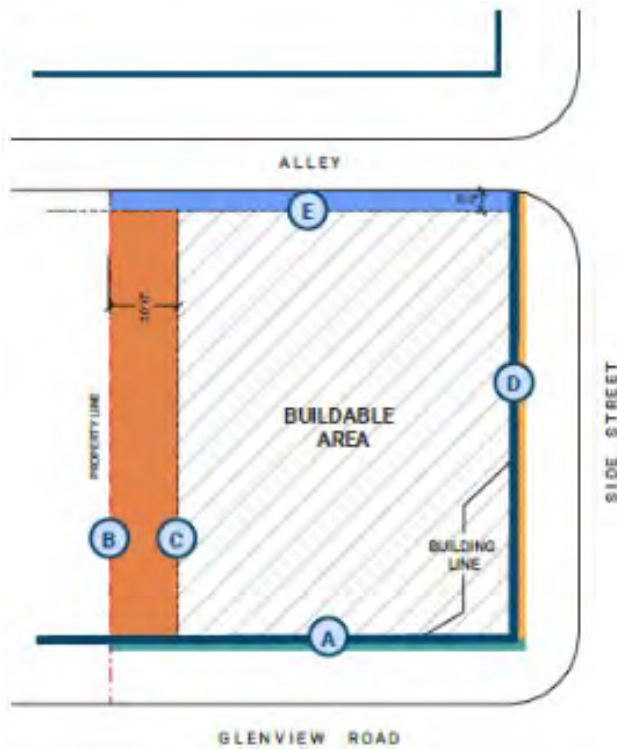
1. *Front yard.* Zero. At least 75 percent of the ground-floor front façade must be built to within 30 inches of the building line, and ground-floor façades must be built to within 30 inches of the building line at each side property line. Within the 25 percent of the ground-floor façade not built to the building line, elements such as pocket parks, plazas, outdoor dining, areas to preserve mature trees, and landscape areas should be included. Where the façade above a recessed building entrance maintains the "street wall" at a distance within

30 inches of the building line, the frontage of the recessed entrance at the ground level shall be considered to be on the building line.

2. *Side yard, interior.* Zero, at ground level. A ten-foot pedestrian pass-through is allowed if necessary to access a rear parking lot.
3. *Side yard, interior adjacent to residential district (including transitional street frontage).* Twenty feet minimum
4. *Side yard, corner lot on side street.* At least 60 percent of the ground-floor side façade must be built to within 30 inches of the building line.
5. *Rear yard.* Five feet minimum.

Figure d-10: Glenview Rd.—Building Setbacks

This diagram represents typical conditions; existing conditions at individual sites may vary.



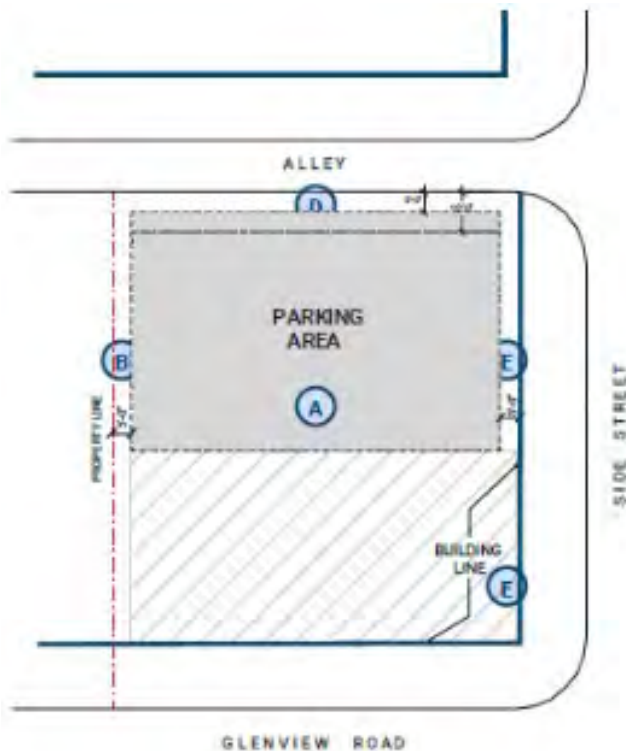
d. *Building step-backs.*

1. *Front:* Starting with the fourth floor, ten feet from the building line, except for corner elements within 50 feet of the corner of the building.
2. *Side, interior:* Starting with the second floor, ten feet from property line to allow air and light to windows.
3. *Side, corner lot:* Starting with the fourth floor, ten feet.
4. Building step-backs can include terrace step-backs, wedding cake step-backs, or corner terraces. See subsection (e)(7)b., Building massing strategies, for more detail.

- e. *Parking placement.* The following standards are for off-street, surface parking lots. In addition, parking may be contained within buildings or underground.
1. Off-street, surface parking lots and parking decks must be placed behind a building.
 2. Five feet from the side yard. If a side alley or access drive is present, parking is permitted within the five-foot setback if the spaces are perpendicular to the alley. (Adjacent commercial parking lots must be connected.)
 3. Ten feet minimum from the rear of the lot if not adjacent to an alley.
 4. Five feet minimum from the rear of the lot if adjacent to an alley.
 5. Five feet from the building line on the side yard of a corner lot.
 - i. *Standards for underground parking.* Underground parking must meet the building setbacks for the front and side yards at ground level specified in subsection (d)(6)c and the parking placement standards for the rear yard specified in subsection (d)(6)e.

Figure d-11: Glenview Rd. - Parking Placement

This diagram represents typical conditions; existing conditions at individual sites may vary.



- f. *Access.*
1. Mid-block curb cuts and access drives are not allowed in the Glenview Road Corridor. Loading, if provided, and parking access must be from the alley or side street.
 2. If no alley exists at the time of development, the developer is required to dedicate an alley ROW, construct the alley pursuant to Village requirements, maintain it by keeping it clear

of stored materials and vehicles, and provide landscaping adjacent to the alley. Alley ROW width must be 20 feet with five feet remaining in the setback for foundation landscaping.

- g. *Use.* Land uses shall be in accordance with section 98-135(c)(6). For a list of specific permitted and conditional uses, see section 98-84.
- h. *Building types.* Building types allowed in the Glenview Road Corridor shall be retail/office and mixed-use as defined in subsection (e), Design Standards.

(7) *Waukegan Road Corridor.*

- a. *Purpose and intent.* The Waukegan Road Corridor is currently an auto-oriented roadway that provides an entrance into Downtown Glenview's main shopping area. South of Grove Street, this corridor is envisioned as an extension of the pedestrian-oriented shopping environment found on Glenview Road. North of Grove Street, this corridor should provide an attractive, mixed-use gateway into the downtown retail core.

The Development Code seeks to both accommodate auto traffic and enhance the pedestrian environment along Waukegan Road. Establishing a consistent "streetwall" with an ample sidewalk/streetscape is the main priority for this frontage. Following a common building line and prohibiting off-street parking in front of buildings will allow Waukegan Road to be "framed" over time with building "streetwalls": (See Figure d-1.)

- b. *Street/sidewalk standards.*

WAUKEGAN Rd. ROW Segments	TOTAL ROW	
Glenview Rd. to Dewes St.	110'	Figure d-12
East Lake Ave. to Glenview Rd.	110'	Figure d-13



This table represents typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figure d-12: Waukegan Road: Glenview Rd. to Dewes St.

Figure represents conditions near Glenview Road while conditions reverse near Dewes St.

WAUKEGAN ROAD: GLENVIEW RD. TO DEWES ST.

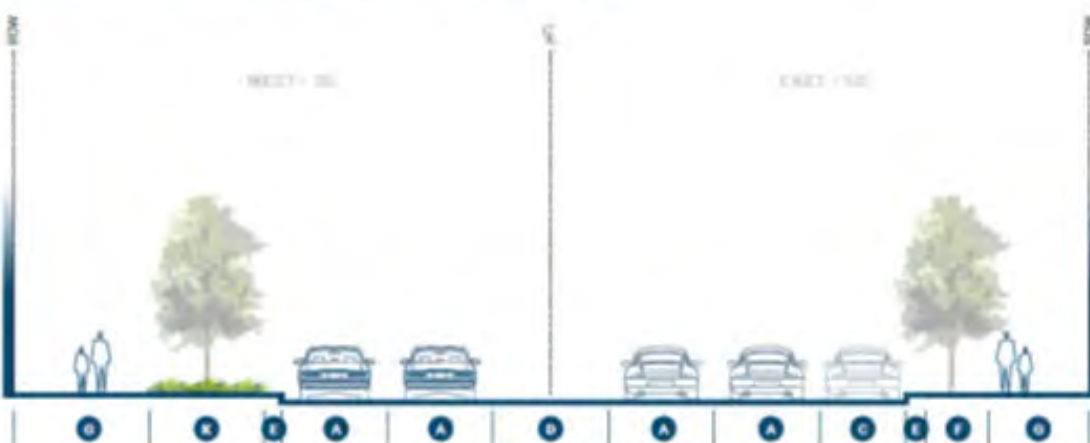
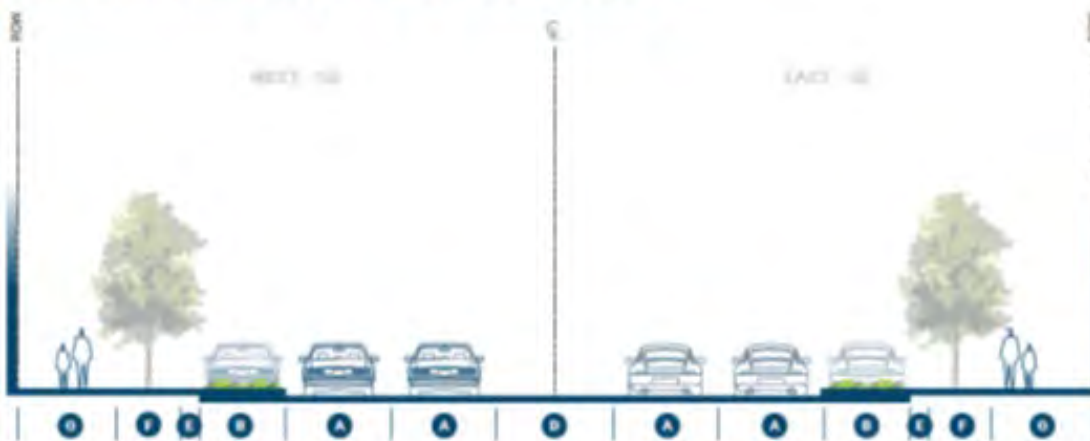


Figure d-13: Waukegan Road: East Lake Ave. to Glenview Rd.

WAUKEGAN ROAD: EAST LAKE AVE. TO GLENVIEW RD.



c. *Building setbacks.*

1. *Front yard:* Zero. At least 60 percent of the ground-floor front façade must be built to within 30 inches of the building line, and ground-floor façades must be built to within 30 inches of the building line at each side yard property line (unless an access drive is to be built adjacent to the interior side property line). An additional ten-foot plus 30 inches landscape setback is allowed for residential buildings. Within the 40 percent of the ground-

floor façade not built to the building line, elements such as pocket parks, plazas, areas to preserve mature trees, and landscape areas should be included. Recessed building entries that maintain the "street wall" above do not count against building line calculations.

2. *Side yard, interior:* Zero, at ground level. A ten-foot pedestrian pass-through is allowed if necessary to access a rear ROW lot.
3. *Side yard, adjacent to an access drive within property:* Ten feet minimum where there is no alley requirement, zero in all other locations.
4. *Side yard, corner lot on the side street:* At least 60 percent of the ground-floor side façade must be built to within 30 inches of the building line.
5. *Rear yard, adjacent to an alley:* Five feet minimum.
6. *Rear yard, not adjacent to an alley:* Twenty-five feet minimum.

Figure d-14: Waukegan Rd. - Building Setbacks

This diagram represents typical conditions; existing conditions at individual sites may vary.



- d. *Building step-backs.*
 1. Side, interior: Starting with the second floor, ten feet from property line to allow air and light to windows.
 2. Side, corner lot: Starting with the fourth floor, ten feet.
 3. Rear: Starting with the fourth floor, ten feet.
 4. Building step-backs can include step-backs, wedding cake step-backs, or corner terraces. See subsection (e)(7)b, Building massing strategies, for more detail.
- e. *Parking placement.* The following standards are for off-street, surface parking lots. In addition, parking may be contained within buildings or underground.

-
1. All residential buildings.
 - A. Off-street, surface parking must be placed in the rear 50 percent of the lot depth (from the front building line to the rear property line) except on the west side of Waukegan between the river and Dewes St.
 - B. Five feet from the interior side yard (adjacent parking lots must be connected when sharing access drive).
 - C. Ten feet minimum from the rear of the lot if not adjacent to an alley.
 - D. Five feet minimum from the rear of a lot if adjacent to an alley.
 - E. Five feet from the building line on the side yard of a corner lot.
 2. Retail/mixed-use buildings less than 30,000 square feet on ground floor.
 - A. Off-street, surface parking is encouraged to be placed in the rear of the building, and at least 75 percent of the spaces must be placed in the rear 75 percent of the lot depth (from the front building line to the rear property line).
 - B. Five feet from the building line for the remaining 25 percent of spaces. The parking area may not occupy more than 50 percent of the frontage.
 - C. Five feet from the interior side yard (adjacent commercial parking lots must be connected).
 - D. Five feet minimum from the rear if adjacent to an alley.
 - E. Ten feet minimum from rear if not adjacent to an alley, or if adjacent to properties along a transition street or other residential zone.
 - F. Five feet from the building line on side streets.
 3. Retail/mixed-use buildings greater than 30,000 square feet on ground floor.
 - A. Five feet minimum from the front building line, although a portion of the off-street parking is encouraged to be placed to the side or rear of the building.
 - B. Five feet from an adjacent rear alley.
 - C. Fifteen feet from the rear setback if no rear alley exists, or if adjacent to a residential zone.
 4. Standards for parking enclosures: Sites without alleys are permitted to have an enclosed parking within the rear yard building setback. The parking enclosure must be setback ten feet from the rear lot line and the structure cannot be more than ten feet in height. Covered commercial parking is encouraged to be open and not enclosed with walls.
 5. Standards for underground parking: Underground parking must meet the building setbacks for the front and side yards at ground level specified in subsection (d)(7)c and the parking placement standards for the rear yard specified in subsection (d)(7)e.

Figure d-15: Waukegan Rd.—Residential Surface Parking Placement

This diagram represents typical conditions; existing conditions at individual sites may vary.

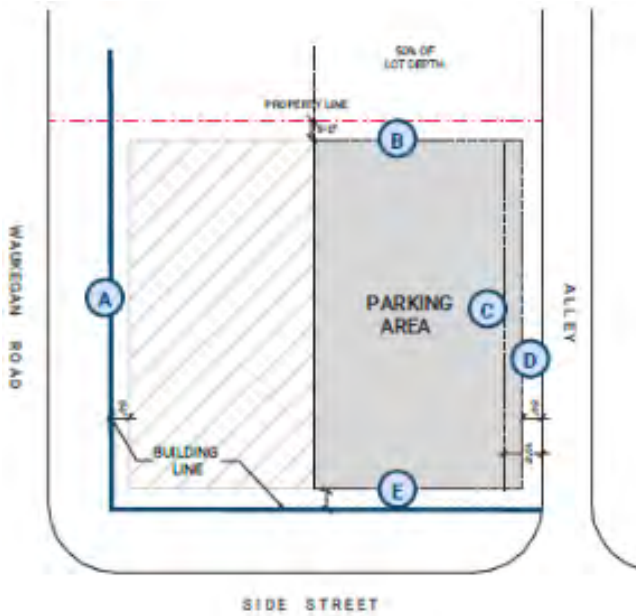


Figure d-16: Waukegan Rd. - Residential Surface Parking Placement, Retail/Mixed-Use less than 30,000 SF

This diagram represents typical conditions; existing conditions at individual sites may vary.



Figure d-17: Waukegan Rd. - Surface Parking Placement, Retail/Mixed-Use greater than 30,000 SF

This diagram represents typical conditions; existing conditions at individual sites may vary.



f. Access.

1. Mid-block curb cuts and access drives are not allowed in the Waukegan Road Corridor north of Grove Street and south of Lake Street on the west side of Waukegan Road. Loading, if provided, and parking access must be from the alley or side street.

If no alley exists at the time of development, the developer is required to dedicate an alley ROW, construct the alley pursuant to Village requirements, maintain it by keeping it clear of stored materials and vehicles, and provide landscaping adjacent to the alley. Sufficient ROW should be dedicated to provide a minimum alley width of 20 feet, with five feet remaining in the setback for foundation landscaping. Access to alley from single-family lots on west side is prohibited. Property under common ownership bisected by alley ROW can be used for parking purposes. (See Figure d-18 and Figure d-19)

Figure d-19: Future Alley North of Grove Street and South of Maplewood Lane



Any mid-block developer is required to build the alley up-front in anticipation of future links. The site plan must show how parking could be connected to the alley in the future and how a temporary Waukegan Road driveway would be disconnected and greened when the alley becomes active.

Because the mid-block access drive is a temporary condition until a continuous rear alley is established, upper stories are permitted to be built to the side lot line over the access drive.

2. Curb cuts and access drives are allowed on all other Waukegan Road blocks and shall be shared where feasible. Adjacent surface parking lots utilized by commercial uses must be connected.
3. Curb cuts along Waukegan Road must be at least 100 feet from any intersection with a side street, such as Stevens, Maplewood, MacLean, Grove, and Glenview.

4. Direct access to indoor parking, such as a parking deck or indoor parking within a residential or mixed-use development, is not allowed directly off of Waukegan Road. Access must be from a side street or access drive, alley, or parking area. Temporary access can be permitted from Waukegan Road until alley is improved to nearest adjacent street.
 5. Access drives must be shared with other mid-block developments on the same block.
 6. If a driveway is to be used to access commercial or residential rear parking lots on Waukegan Road, it must be shared with future developments on adjacent properties via easements if the future development cannot be accessed from an alley or side street.
 7. Access driveways to indoor parking as well as surface parking lots from side streets must be at least 25 feet from Waukegan Road.
- g. *Use.* Land uses shall be in accordance with section 98-135(c)(6). For a list of specific permitted and conditional uses, see section 98-84.
- h. *Building types.* Building types allowed in the Waukegan Road Corridor shall be retail/office, mixed-use and apartment/condominium as defined in subsection (e), Design standards.
- i. *Development scenarios.* The following 3-D massing diagrams show potential development scenarios on a sample block along the Waukegan Road Corridor.

Figure d-20 shows two infill developments that comply with the Downtown Development Code and fit in among existing buildings. Figure d-21 shows how the block could develop over time with four buildings. In all cases, developments should feature high-quality architecture with appropriate articulation and fenestration as discussed in subsection (e), Design standards.

Figure d-20: Infill Redevelopment



Figure d-21: Infill Redevelopment—Quarter-block Segments



(8) *Secondary street.*

- a. *Purpose and intent.* Dewes Street, Railroad Avenue, and Harlem Avenue as well as side streets that intersect with Glenview and Waukegan Roads are considered secondary streets within downtown. These streets are intended to have a mixed-use character that is supportive of the pedestrian-oriented shopping environment along the Glenview Road and Waukegan Road Corridors. The secondary street frontage also seeks to provide appropriate transitions from the shopping blocks to adjacent residential neighborhoods.

The Downtown Revitalization Plan and Development Code encourage a consistent "streetwall" of buildings and ample sidewalk/streetscape space. Limited curb cuts are allowed to provide shared access drives to new developments within a block. (See Figure d-1, as well as figures throughout this subsection).

- b. *Street/sidewalk standards.*

SECONDARY STREETS ROW Segments	TOTAL ROW	Figure
Harlem Ave.	100'	Figure d-22
Lehigh Ave.	90'	Figure d-23
Dewes St.	60'	Figure d-24
Depot St.	50'	Figure d-25
Railroad Ave.	60'	Figure d-26
Typical Residential Street	60'	Figure d-27



This table represents typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figure d-22: Secondary Street: Harlem Ave.

SECONDARY STREET: HARLEM AVE.

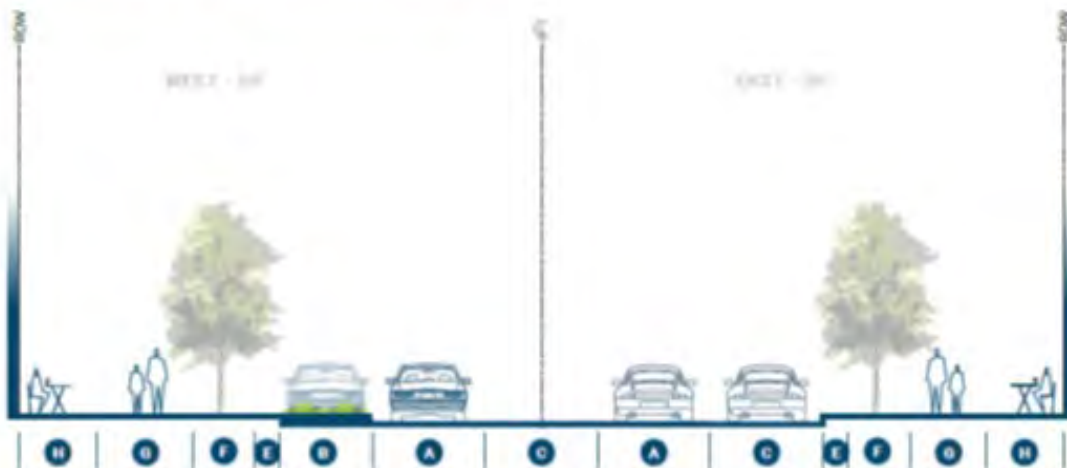
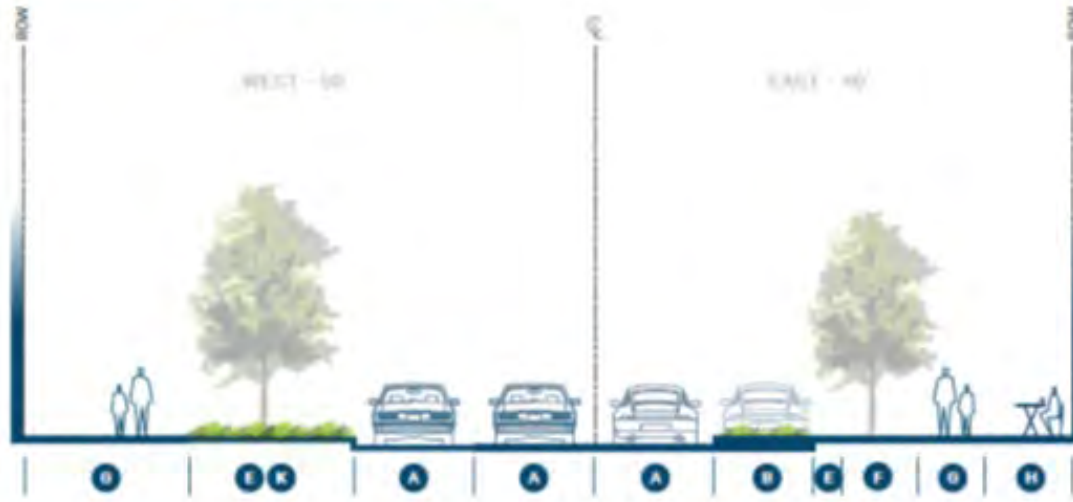


Figure d-23: Secondary Street: Lehigh Ave.

SECONDARY STREET: LEHIGH AVE.



	DEWES ST.		DEPOT ST.	
	60'	50'		
	East	West	East	West
TOTAL ROW	30'	30'	-	-
A TRAVEL LANES	11'-0"	11'-0"	-	-
B PARALLEL PARKING/ CURB BUMP OUT	-	-	9'-0"	-
C TURN LANE	-	-	-	-
D MEDIAN	4'-0"	-	-	-
E CURB + CARRIAGE WALK	0'-6"	2'-0"	2'-6"	-
F PLANTING ZONE	4'-0"	6'-0"	5'-0"	-
G SIDEWALK	5'-0"	9'-0"	5'-0"	6'-0"
H DINING ZONE	-	-	-	-
I ANGLED PARKING	-	-	-	-
J HEAD-IN PARKING	-	-	18'-0"	-
K PARKWAY	-	-	-	-

This table represents typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

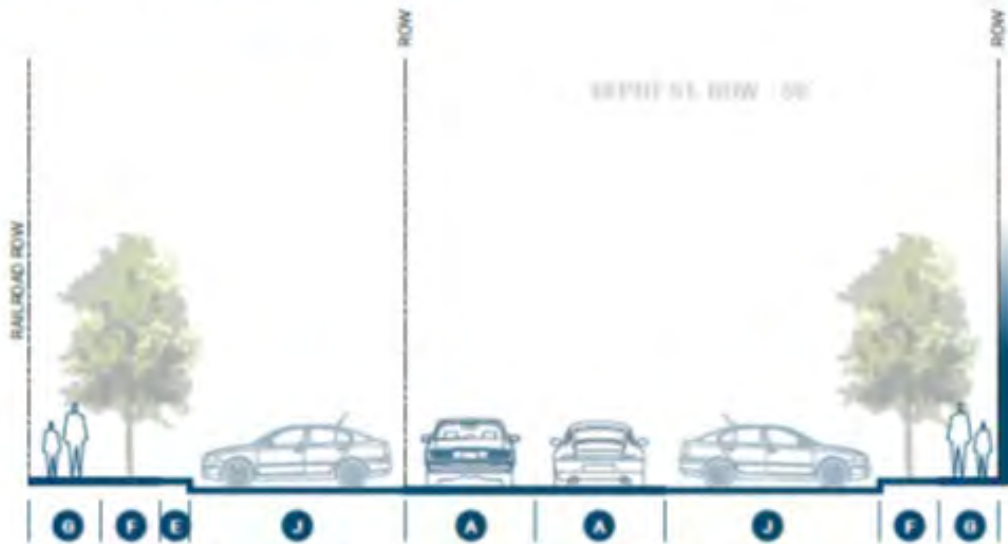
Figure d-24: Secondary Street: Dewes St.

SECONDARY STREET: DEWES ST.



Figure d-25: Secondary Street: Depot St.

SECONDARY STREET: DEPOT ST.





This table represents typical conditions. Final dimensions may vary depending on existing conditions and will be reviewed on a case by case basis as part of the Development Review and Engineering Review processes.

Figure d-26: Secondary Street: Railroad Ave.

SECONDARY STREET: RAILROAD AVE.

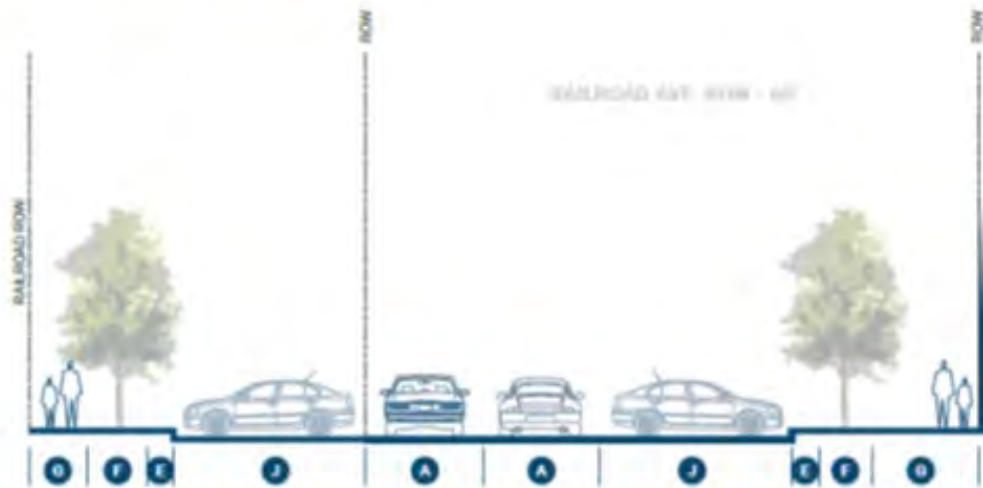
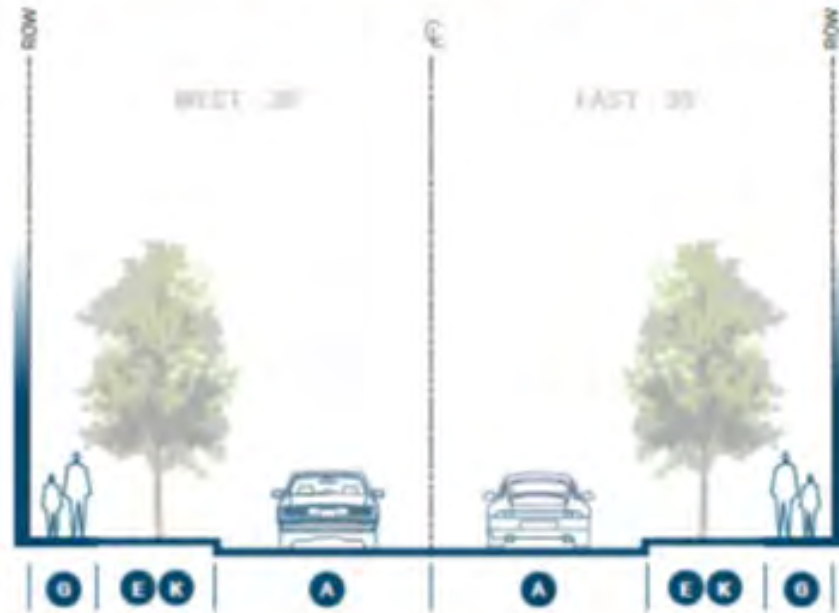


Figure d-27: Secondary Street: Typical Residential Street.

On-street parking typically allowed on one side of the street of the street within the 26' travel lane section.

SECONDARY STREET: TYPICAL RESIDENTIAL STREET



c. *Building setbacks.*

1. Front yard: Zero. At least 60 percent of the ground-floor front façade must be built to within 30 inches of the building line, and ground-floor façades must be built to within 30 inches of the building line at each side yard property line. An additional ten-foot landscape setback is allowed for residential buildings. Within the 40 percent of the ground-floor façade not built to the building line, elements such as pocket parks, plazas, areas to preserve mature trees, and landscape areas should be included. Recessed building entries that maintain the "street wall" above do not count against building line calculations.
2. Side yard, interior: Zero, at ground level. A ten-foot pedestrian pass-through is allowed if necessary to access a rear parking lot.
3. Side yard, adjacent to an access drive: Ten feet.
4. Side yard, interior adjacent to residential district (including transitional street frontage): Twenty feet minimum.
5. Side yard, corner lot on side street: At least 60 percent of the ground-floor side façade must be built to within 30 inches of the building line.
6. Rear yard, adjacent to an alley: Five feet minimum.
7. Rear yard, not adjacent to an alley: Ten feet minimum. If a ten-foot rear step-back is included above the third floor, only a ten-foot minimum rear setback is required.

Figure d-28: Secondary St. - Building Setbacks

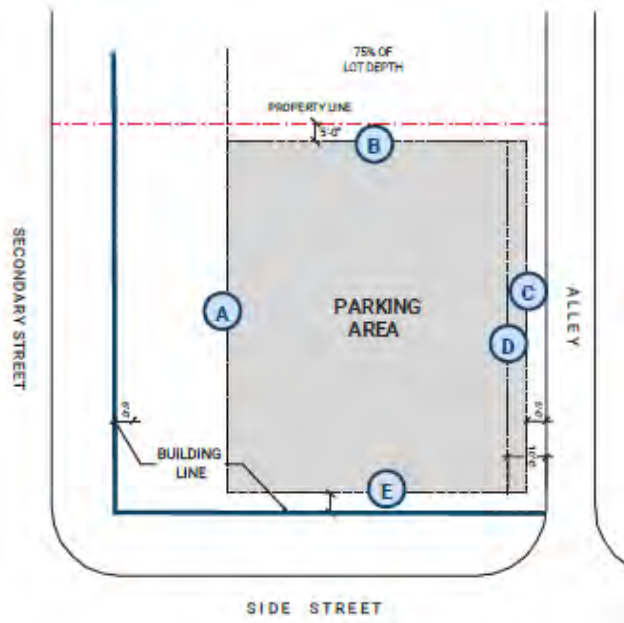
This diagram represents typical conditions; existing conditions at individual sites may vary.



- d. *Building step-backs.*
1. Front: Starting with the fourth floor, ten feet from the building line.
 2. Side, interior: Starting with the second floor, ten feet from property line must be included to allow air and light to windows. (See Figure d-28.)
 3. Side, corner lot: Starting with the fourth floor, ten feet.
 4. Building step-backs can include step-backs, wedding cake step-backs, or corner terraces. See subsection (e)(7)b, Building massing strategies, for more detail.
- e. *Parking placement.* The following standards are for off-street, surface parking lots. In addition, parking may be contained within buildings or underground.
1. Off-street, surface parking not contained within the building or underground must be placed in the rear 75 percent of the lot depth (from the front building line to the rear property line).
 2. Five feet from the interior side yard (adjacent commercial parking lots must be connected).
 3. Five feet from the rear if adjacent to a rear alley.
Ten feet from the rear if not adjacent to an alley or if adjacent to properties along a transition street frontage or adjacent to other residential districts.
Five feet from building line on the side yard of a corner lot.
- i. Standards for underground parking: Underground parking must meet the building setbacks for the front and side yards at ground level specified in subsection (d)(8)c and the parking placement standards for the rear yard specified in subsection (d)(8)e.

Figure d-29: Secondary St. - Parking Placement

This diagram represents typical conditions; existing conditions at individual sites may vary.



- f. *Access.*
 - 1. Curb cuts are allowed on secondary street frontages but must be spaced at least 85 feet apart.
 - 2. Curb cuts and access drives shall be shared where feasible, and adjacent commercial surface parking lots must be connected.
 - 3. Curb cuts must be at least 25 feet from any intersecting street.
 - g. *Use.* Land uses shall be in accordance with section 98-135(c)(6). For a list of specific permitted and conditional uses, see section 98-84.
 - h. *Building types.* Building types allowed on the secondary street shall be retail/office, mixed-use, condominium/apartment as defined in subsection (e), Design standards.
- (9) *Transition street.*
- a. *Purpose and intent.* The transition street frontage provides a residential zone that fits the downtown character of its location and provides a transition to residential neighborhoods to the south and west.
 - b. *Street/sidewalk standards.* The building line in the transition street corridor frontage provides a larger sidewalk to allow for increased landscape and buffering across from established residential uses. The optimal ROW is 86 feet.
 - c. *Building setbacks.*
 - 1. Front yard: Zero. At least 75 percent of the front façade must be built to within 30 inches of the building line. An additional ten-foot landscape setback is allowed for residential buildings.
 - 2. Side yard, interior: Ten feet minimum from a lot line or access driveway.

-
3. Side yard, corner lot on side street: At least 60 percent of the ground-floor side façade must be built to within 30 inches of the building line.
 4. Rear yard: Ten feet minimum.
- d. *Parking placement.* Townhomes and rowhomes must have two indoor parking spaces per unit (within unit or in a garage). Minimum setbacks for any additional surface parking are:
1. Twenty-five feet from the front building line.
 2. Five feet from the side street building line.
 3. Fifteen feet from an interior side yard lot line or access drive.
 4. Five feet from the alley or rear lot line.
 5. Standards for underground parking: Underground parking must meet the building setbacks for the front and side yards at ground level specified in subsection (d)(9)c. and the parking placement standards for the rear yard specified in subsection (d)(9)d.
- e. *Access.*
1. Curb cuts are allowed on transition street frontages but must be spaced at least 85 feet apart.
 2. Curb cuts and access drives shall be shared where feasible.
 3. Curb cuts must be at least 25 feet from any intersecting street.
 4. Parking and services shall be accessed from an alley or garage. Rowhomes and townhomes are not allowed on a lot without an alley.
 5. Garages on corner lots shall face the alleys.
- f. *Use.* Land uses shall be in accordance with section 98-135(c)(6). For a list of specific permitted and conditional uses, see section 98-84.
- g. *Building types.* Building types allowed on transition street shall be townhomes and rowhomes as defined in subsection (e), Design standards.
- (10) *River Corridor.*
- a. *Purpose and intent.* In addition to a green edge along the West Fork of the North Branch of the Chicago River, the Downtown Revitalization Plan envisions a central open space framed by mixed-use buildings south of Glenview Road. The River Corridor Frontage requirements provide appropriate setbacks from the river to create an attractive, landscaped open space along its edge.
- b. *Requirements.*
1. Buildings and parking must be at least 35 feet from the edge of the river, which is measured from the established high-water elevations - in most cases the channel bank.
 2. Property owners shall accommodate public access to the river through paths, plazas, gardens, or other forms of open space.
 3. Natural landscaping should be provided within the 35-foot setback.
 4. Developments at this location should comply with the standards for the street onto which they front and should also review the standards for the adjacent street frontage.

- 5. If in accordance with the vision presented in the 2021 Strategic Plan, River Drive, south of Glenview Road, is vacated by the Village and a park is added between the river and new development, the building line for any new development west of the park will be located 10 feet away from the new property line. The size and location of the park (if any) has not been determined but Figure d-30 and Figure d-31 reflect a range of possible outcomes as determined by the 2021 Downtown Strategic Plan. Within the ten-foot setback of a new building line from the property line, elements such as pocket parks, plazas, areas to preserve mature trees, and landscape areas should be included. These features should complement the adjacent park if it has been designed or installed.

Figure d-30: Balanced Park Option A



Figure d-31: Balanced Park Option D



The Park scenarios presented in this document only reflect a range of possible outcomes as determined by the 2021 Downtown Strategic Plan.

(e) *Design standards.*

- (1) *Purpose and intent.* The following design standards were created to complement the development regulations in the Downtown Development Code. The Standards should be used by landowners, businesses, developers, planners, architects, landscape architects, and engineers as guiding principles for developments proposed in Downtown Glenview. The design standards are intended to maintain and enhance the physical character of downtown by encouraging development proposals that strive for high-quality design.
- (2) *Village character.* Although these standards do not mandate or endorse one particular style of architecture, they do recognize the value of Downtown Glenview's traditional historic revival architecture and its identity as an upscale, North Shore commuter railway village. The standards encourage development that respects the village's history, "fits in" with high-quality adjacent structures, and enhances downtown's overall architectural character.

In light of Glenview's extensive and popular public land holdings, the community's history of preserving, creating, and supporting unique natural areas, and the village's award-winning professional natural resources staff, the preservation of existing and installation of new trees and green infrastructure in the downtown will require forethought, strategy, and employing available technologies from the scientific fields of arboriculture and urban forestry.

- (3) *Objectives.* Site and building design should enhance downtown's eclectic character by:
 - Fostering a "built over time" appearance with variations in façades, building height, and rooflines.
 - Articulating buildings with detailed fenestration, high-quality materials, signage, and lighting.
 - Incorporating attractive, year-round streetscape/landscape elements.

-
- Incorporating environmentally sensitive, sustainable design where feasible.

Using forethought, strategy, science, and technology, we strive to create, maintain, and foster a mature tree canopy in the downtown for the benefit of businesses, residents, and visitors for generations to come.

- (4) *Context.* The design of each development, whether it consists of one or multiple buildings, will be reviewed by Village boards and commissions for its suitability for a given site, compatibility with adjacent development, and consistency with the goal of improving the design quality of Glenview's built environment. In addition, other plans and development regulations affect development in Downtown Glenview.

The Comprehensive Plan provides goals and objectives and a land-use direction for the entire Village.

The 2006 Downtown Revitalization Plan and 2021 Downtown Strategic Plan sets a new direction for the improvement and development of the area. The Downtown Development Code and its design standards seek to implement the Revitalization Plan by providing development regulations and design direction.

The Glenview Municipal Code provides a variety of other related development regulations, as noted in the introduction and administration sections.

- (5) *Organization.* Section 98-135(e)(6) lists the standards specific to certain building types in the downtown and includes "wireframe" diagrams that provide guidance regarding appropriate scale and massing. This section also provides photographs of buildings that feature high-quality design.

Subsection (e)(7) provides design standards applicable to all downtown buildings on properties covered by the code.

Subsections (e)(8) through (e)(10) provide standards for building materials, lighting, and outdoor cafés.

Subsection (e)(11) provides parking standards.

Subsection (e)(12) includes standards for downtown streetscape and landscape.

- (6) *Building types.* The following building types are allowed in Downtown:

- Mixed-use
- Retail/office
- Multi-family
- Rowhome/townhome

Each building type must comply with the regulating framework and street frontages sections and follow the design standards in this section.

The photos and graphics that follow show images for each building type. They provide additional direction for building siting, massing, and design. The 3-D "wireframe" diagrams are intended to show massing, scale, and fenestration but are not intended to show detailed architecture and articulation or indicate a preference for one architectural style for downtown.

Photographs included in these design standards are intended to show high-quality design and architecture but may not comply with every provision of the Downtown Development Code.

a. *Mixed-Use.*

1. Mixed-use building on corner lot.

Figure e-1: Mixed-Use Building on Corner Lot



2. Mixed-use building, mid-block with access drive.

Figure e-2: Mixed-Use Building with Access Drive



3. Mixed-use building, mid-block without access drive.

Figure e-3: Mixed-Use Building, Mid-block without Access Drive



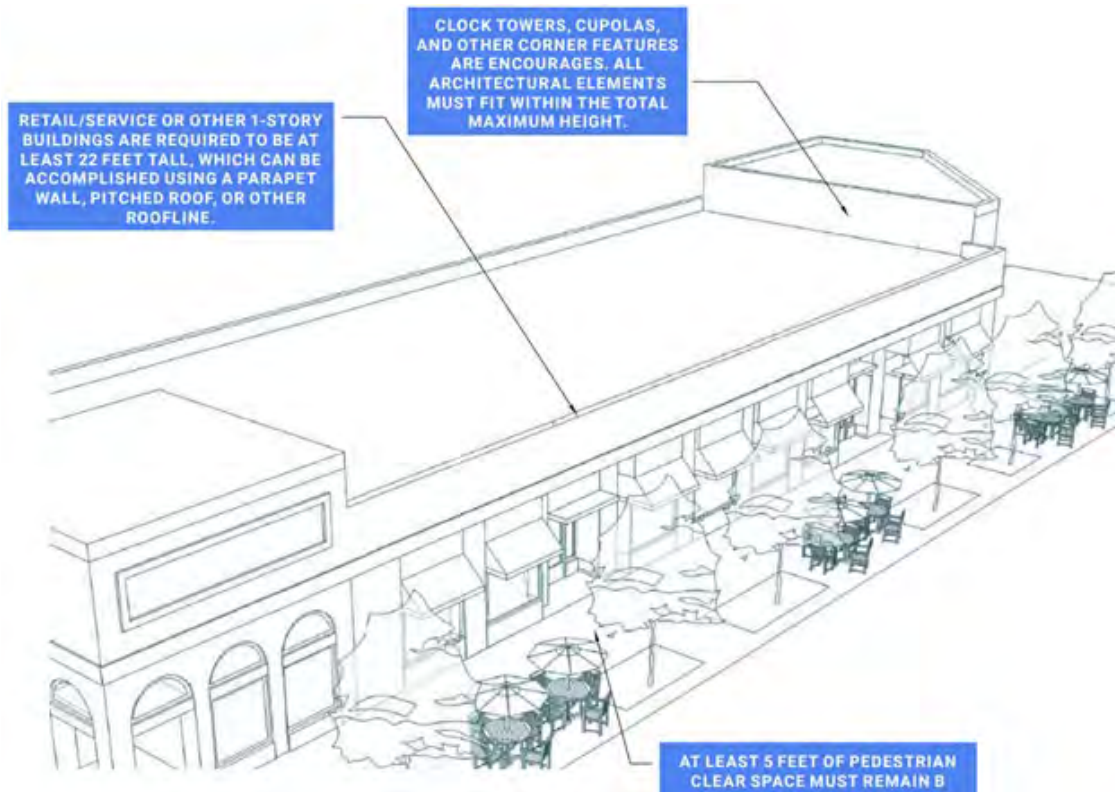
- 4. Mixed-use building incorporating varied roof features.

Figure e-4: Mixed-Use Building Incorporating Varied Roof Features



- b. Retail/office.

Figure e-5: Retail/Office-only Building



c. *Multi-family.*

Figure e-6: Condominium/Apartment Building



d. *Rowhome/townhome.*

Figure e-7: Rowhome/Townhome



1. The main entrance to each dwelling must face the street.
2. Buildings on corner lots shall be designed with two front façades.
3. Service, equipment, and trash areas must be located in and accessed from the alley where feasible.
4. The design of garages, including materials, should be compatible with the design of the rowhomes/ townhomes.

(7) *Building design.*

a. *General.*

1. To maintain an active pedestrian environment, buildings shall be oriented toward streets, sidewalks, and/or public plazas.
2. Façades shall be proportioned to respect the human scale.
3. Façade elements shall provide a change in plane, creating interest in light and shadow.
4. Standardized, formulaic, non-regional architecture and architectural features used primarily for advertising purposes are not allowed.
5. Developers should consider creative, adaptive reuse of high-quality existing buildings.

- b. *Building massing strategies.* The following building massing strategies are intended to reduce the visual mass of building façades, reduce the "canyon effect" on streets, while maintaining a "street wall" that is crucial to creating a pedestrian-oriented environment. Any building up to two floors

with a façade over 120 feet long, and all buildings over two floors shall utilize façade articulation (see subsection (e)(7)b1. and all buildings over three stories shall require at least one other building massing strategy. This is in addition to specific requirements for street frontages specified in subsection (d), Street frontages.

1. *Façade articulation.* Façade articulation is a strategy for creating rhythm and visual interest on a building façade or expressing a corner element, and can be used in conjunction with other building massing strategies. Articulated façades shall not be set back less than 30 inches from the primary building façade. Façade articulation is required based on a 40-foot average, minimum. For example, a building that is 160 feet long would require four articulation points that could be more than six feet wide each on the façade.

Articulated façades are not required to be expressed on the ground level of a mixed-use building along a commercial tenant space, as the recessed tenant space entry serves this function. The articulation setback areas will be calculated according to the building line standards described in each frontage zone building setbacks subsection. See Figure e-8.

Figure e-8: Façade Articulation



2. *Wedding cake step-back.* A wedding cake step-back is similar to a terrace step-back, except the step-back occurs on the upper floor rather than the second. This strategy should be considered to reduce the visual impact of buildings on the street and adjacent properties. Typically wedding cake step-backs are utilized on front or side façades but could also be used on rear façades to minimize the visual impact of buildings on adjacent properties. All step-back areas are encouraged to be utilized as public or private terraces and shall be set back no less than ten feet and no greater than 15 feet from the ground level façade. 75 percent of a building's upper floor façade must be stepped back to be considered a wedding cake step-back. See Figure e-9.

Figure e-9: Wedding Cake Step-back



3. *Corner terrace.* A Corner Terrace is a public or private terrace located at a building corner. They are typically located above the first floor and serve as an amenity space for residential or office or outdoor dining terraces. Corner terraces shall be a minimum of 400 square feet and no larger than 25 percent of the ground floor building footprint. See Figure e-10.

Figure e-10: Corner Terrace



4. *Terrace step-back.* A terrace step-back is defined as a setback of the building façade above the first floor. Typically terrace step-backs are utilized on front or side façades but could also be used on rear façades to minimize the visual impact of buildings on adjacent properties. All step-back areas are encouraged to be utilized as public or private terraces and shall be set back no less than ten feet and no greater than 15 feet from the ground level façade. 75 percent of a building's upper level façade must be stepped back to be considered a terrace step-back. See Figure e-11.

Figure e-11: Terrace Step-back



c. *Articulation.*

1. *Plane/material changes.*

- i. A building base, middle, and top shall be strongly articulated through materials, details, and changes in the plane of the wall, including step-backs and patios on upper floors.
- ii. Pitched roofs may project into space where upper story floors have been step-backed.
- iii. Mixed-use buildings must have a distinct ground-floor base with easily identifiable, traditional storefronts with clear glass and kneewalls.
- iv. Façades shall be articulated to express vertical rhythm related to structural columns and bays.
- v. Building design should feature a balance of vertical and horizontal elements.
- vi. Unarticulated, flat-front, all-glass, or all-metal buildings are prohibited.
- vii. To prevent a "dead wall" on buildings with first-floor parking, articulate the ground floor with faux windows, banding, or other architectural features.

2. *Visual interest.*

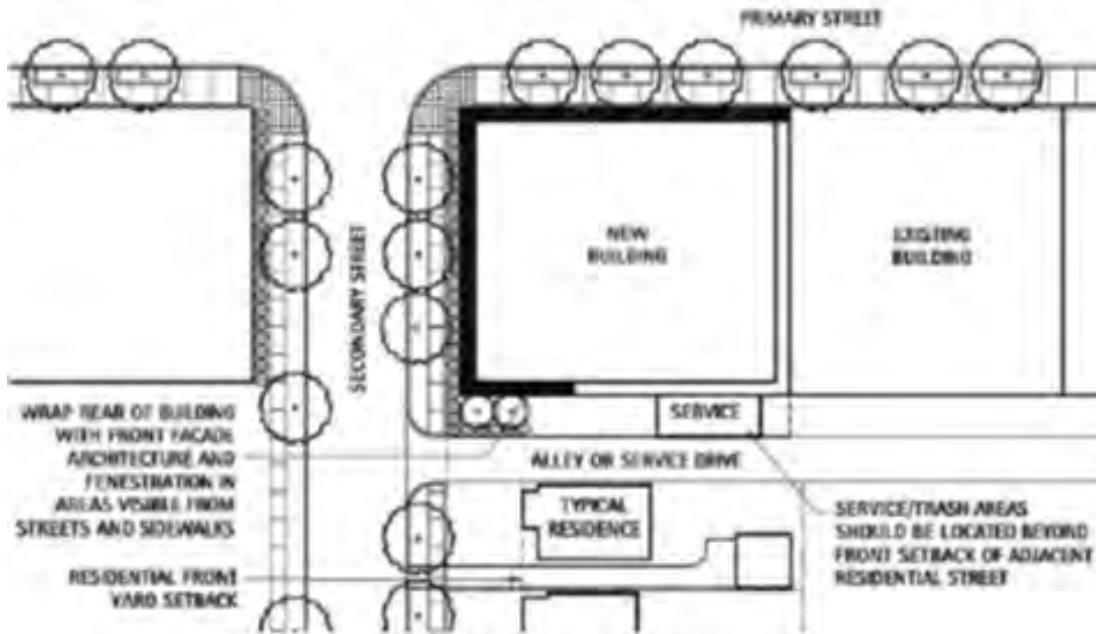
- i. Building orientation and design elements shall encourage overall visual continuity between developments.
- ii. To avoid a look of monotony of materials or design, buildings should be articulated, such as with projections, recesses, material changes, parapets, cornices, and varying roof heights.

-
- iii. Commercial, multi-family, and mixed-use buildings over 150 feet long should be "broken up" with articulation and contribute to a "built over time" appearance. Changes in materials, colors, rooflines, variations of window patterns or modules, architectural styles, or architectural features such as railings, columns, or cornices should also be considered.
 - iv. Because of the prominence and visibility of corner buildings, features such as a cupola, rotunda, atrium, clock tower, pilasters, roofline balustrades, and/or varying rooflines should be considered to add visual interest to downtown.
- d. *Entries.*
- 1. Building entries should be clearly defined and articulated.
 - 2. Primary store entrances shall be located along the primary street frontage, with secondary entrances located behind the building or along a side street.
 - 3. Primary access to individual commercial storefronts, including restaurants, must be from the street/sidewalk and not from inside lobbies and hallways.
 - 4. On mixed-use commercial buildings, residential or office entrances/ lobbies should be distinguished from storefronts and located on side streets and away from intersections wherever possible.
 - 5. Recessed entries for retail and service uses are encouraged to provide cover from the elements and to allow easier opening of doors. Non-recessed doors should not encroach into the five-foot pedestrian clear zone when opened. Primary building entrances shall not be recessed more than ten feet, secondary entrances and storefronts should not be recessed more than five feet. Recessed entries will not count as a portion of the façade built to the building line if the façade above the entry respects the building line. See subsection (d), Street frontages, for more detail regarding setbacks and building line lines.
- e. *Roofs.*
- 1. Roofs can be pitched or flat.
 - 2. Pitched roofs shall utilize a minimum pitch of 4:12 and overhanging eaves shall be incorporated into the design of the building.
 - 3. Flat roofs or pitches below the minimum ratio shall be concealed with a parapet that extends around all sides of the building and shall screen all rooftop mechanical from public view.
 - 4. Varied roof pitches, planes, and heights shall be used to break up the massing of roofs visible from the public ROW. Roofs over 120 feet in length shall incorporate roof variation into the design.
 - 5. The roof design should be incorporated into the overall design of the building, working harmoniously with selected massing strategies specified in subsection (e)(7)b., Building massing strategies.
- f. *Fenestration.*
- 1. *Location.*
 - i. Ground-level retail or office space shall include large, clear-glass windows that allow views into building interiors to reinforce an active shopping and business environment.

-
- ii. Blank walls exceeding 20 feet in length are not allowed facing Glenview or Waukegan Roads.
2. *Amount.*
- i. At least 50 percent of ground-floor retail, service, and office façades facing street frontages or public plazas shall be fenestrated with clear, non-tinted windows.
 - ii. At least 25 percent of every upper-floor façade shall be fenestrated.
 - iii. At least 25 percent of ground-floor façades facing rear parking areas or alleys shall be fenestrated.
 - iv. A kneewall of at least ten inches and not more than 18 inches is required on commercial storefronts.
3. *Materials.*
- i. Fenestration must be either windows or doors that allow views into the shops, working areas, lobbies, or pedestrian entrances or window displays.
 - ii. Dark-tinted, spandrel, frosted, or smoked glass shall be used sparingly and for decorative or accent purposes or on solid walls necessary to the function of the building only (such as storage areas, kitchens, and bathrooms). Reflective glass is prohibited.
 - iii. Solid walls necessary to the function of a building shall incorporate features such as awnings, display windows, material and color variations, arches, piers, columns, high-quality graphics, spandrel glass, landscaping, and other elements to reduce perceived building scale and add visual interest.
- g. *Service areas.*
1. *Design and materials.*
- i. Service areas, pipes, utility boxes, and utility doors should be placed out of view where feasible (see Figure e-12).
 - ii. Accessory service areas behind buildings that are visible from streets and sidewalks shall be designed in a manner consistent with the building front or side.
 - iii. Loading, trash, and utility areas shall be designed to accommodate snow removal by eliminating unnecessary obstacles and providing storage locations where feasible.
 - iv. When parking is located behind buildings, rear building entrances and façades shall be designed in a manner consistent with the front and side façades.
 - v. Access to rear parking lots should be clearly defined and visible from the street.
 - vi. Screening materials should complement the building and adjacent buildings in materials and color and be effective in every season, such as fencing, shrubs, or evergreens. Screening must be at least seven feet in height.
 - vii. For all development that abuts a residential property at the rear, an eight-foot-high solid screen fence is required along the property line. If alley condition is present, fence to be installed within alley ROW adjacent to residential property. Fence to be maintained by property owner.

- viii. Service areas can be used to access underground storm water detention for maintenance, as long as access to driveways or parking areas are maintained.

Figure e-12: Rear Façade Service Area



- 2. *Location and maintenance.*
 - i. Separate areas for loading, trash, and utilities for individual businesses are discouraged. Shared service areas between businesses should be considered for ease of maintenance and improved aesthetics.
 - ii. Rear façades visible from streets and sidewalks should be treated with similar articulated architecture and fenestration as the front and sides of the building. Architecture and fenestration on the rear façade should extend at least 40 feet from the curb on a side street (see Figure e-13).
 - iii. Loading, trash, and utility areas (including pipes, conduit, utility boxes, and utility doors) shall be located out of view wherever possible and in all cases screened from street and sidewalk views. Roof top mechanicals shall be located in the middle of the roof and shielded by a screening wall similar in design and materials to the building. These areas should be incorporated into site plans and building designs.

Figure e-13: Rear Façade Massing

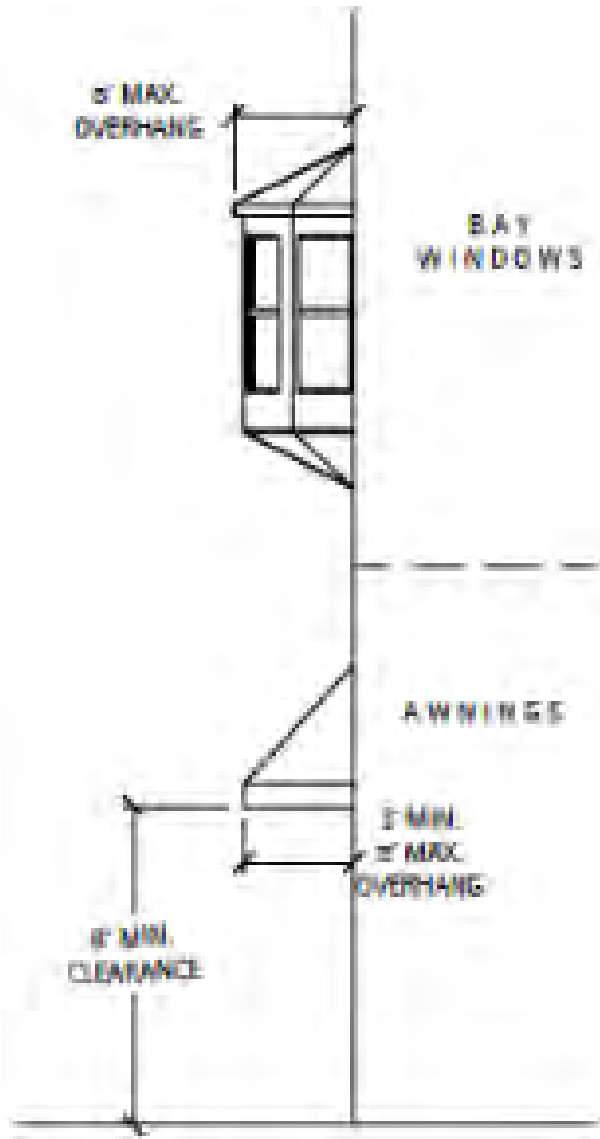
Treat visible rear facades with similar articulated architecture and fenestration as front and sides.



h. *Building projections.*

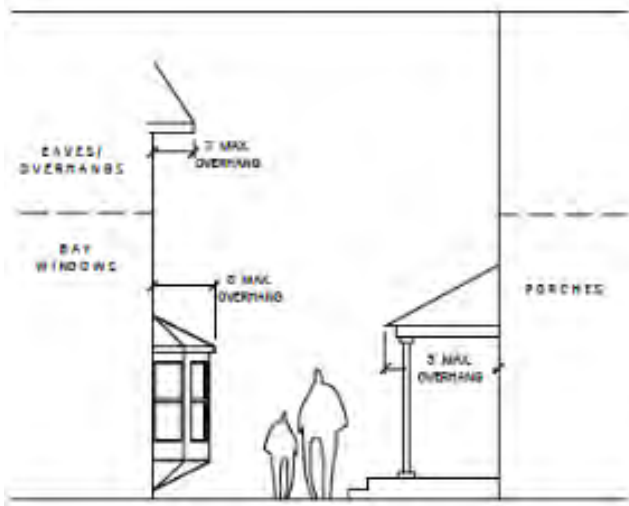
1. *Retail/mixed-use/multi-family.* Eaves and bay or box windows may encroach past the building line, with the following limits (see Figure e-14). For regulations on blade signs and awnings, consult the Village's Zoning Ordinance.
 - i. Eaves: Three feet maximum.
 - ii. Bay/Box windows: Five feet maximum (allowed only on upper stories).

Figure e-14: Projections—Retail, Mixed-Use & Condominiums



2. *Rowhomes/Townhomes.* Eaves, bay windows, and porches or stoops may encroach past the Building Line, with the following limits (see Figure e-15):
 - i. Eaves: Three feet maximum.
 - ii. Bay/Box windows: Five feet maximum.
 - iii. Porches: Five feet maximum.

Figure e-15: Projections—Residential



3. *Balconies.*

- i. Balconies, decks, or terraces must not cross the building line.
- ii. Inset or recessed balconies, decks, or terraces are allowed on the front of buildings.
- iii. Balconies, decks, or terraces are allowed to encroach into areas where the building has been stepped back from the building or property line.

i. *Building colors.*

- 1. Building color shall be compatible with the area's character and enhance the building's visual appeal.
- 2. Primary, bright, or excessively brilliant colors are discouraged unless used sparingly for subtle trim accents.

Table e-1: Preferred/Prohibited Materials

Primary Building Materials
Preferred
Brick Stone Glass Wood
Prohibited*
Concrete, including poured-in-place and unfinished pre-cast Plywood Sheet Pressboard Synthetic stone Smooth/textured synthetic stucco or plaster Metal/decorative metal
* Limited use allowed for decorative accent purposes

(8) *Building materials.*

a. *Overall.*

1. Wood, brick, stone, and glass are the preferred primary building materials for all structures in the downtown. New building construction and rehabilitations should be primarily constructed of these high-quality materials (see Table e-1).
2. Concrete block (smooth or decorative split-face), stucco (smooth or textured synthetic), plywood, sheet pressboard, unfinished pre-cast concrete, poured- in-place concrete, synthetic stone, smooth/textured synthetic stucco or plaster, and metal/decorative metal shall not be used as primary materials on façades or walls that are visible from streets, driveways, sidewalks, and/or parking areas. They shall be used only for decorative accent purposes and limited in their use on building façades and visible walls (see Table e-1).
3. The primary building material used on front façade shall be continued as the primary material on the side and rear façades, except where the side of a building directly abuts the side of an existing building.
4. The number of materials on an exterior building face should be limited (no more than five) to prevent clutter and visual overload.

b. *Fencing.*

1. Brick, stone, or decorative metal shall be used for fencing. Decorative or non-screening fence height shall be 30 to 42 inches. Railings along terraces can be solid walls or open fencing but may not be clear glass or plastic.
2. Chain link fencing is not allowed.
3. Fences shall be considered an extension of building architecture and shall make an attractive transition between the building mass, the natural forms of a site, and the "public realm" or streetscape.

c. *Awnings.* Refer to the Village's Awning Appearance Code and Design Guidelines. In addition, the following guidelines will apply:

1. Building awning design and colors should be consistent and complementary in color, style, and size with the overall building façade and adjacent buildings.
2. Awnings shall be constructed of high- quality fabric or metal. Plastic or vinyl awnings are not allowed. Internally lighted awnings are not allowed.
3. The bottom of awnings shall be placed a minimum of eight feet above the sidewalk.

(9) *Lighting.* Refer to the Municipal Code's performance standards on lighting. In addition, the following requirements will apply:

- a. Site lighting shall incorporate principles to limit "light pollution" and preserve the nighttime environment, such as incorporating down-lighting and shields.
- b. Lighting shall provide a sense of safety without having a negative effect on neighboring properties and shall be located, aimed, or shielded to minimize glare, sky glow, and stray light trespassing across property lines, especially along alleys.
- c. Exterior lighting for signage shall be directed downward.

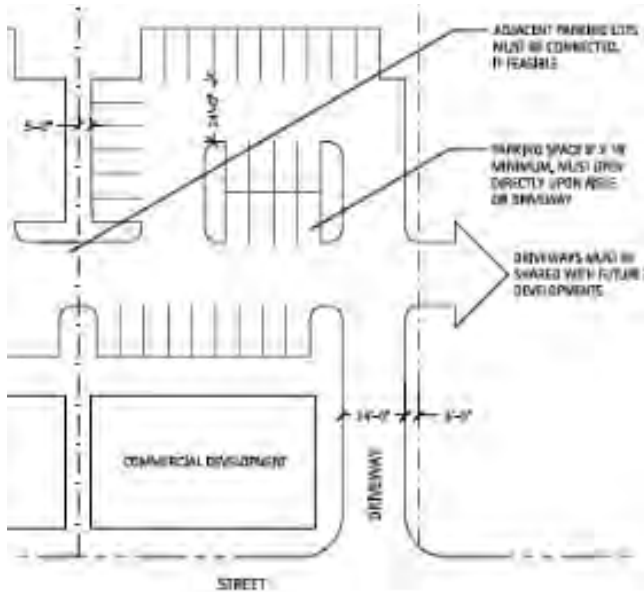
(10) *Outdoor cafés.*

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- a. Outdoor cafés/seating areas are encouraged to make downtown more active and enhance its overall pedestrian character, especially in blocks identified as key commercial/retail/restaurant locations.
 - b. Outdoor cafés shall maintain at least five feet of clear space for movement of pedestrians along the sidewalk.
 - c. Tables, chairs, and other equipment should be kept out of the pedestrian zone. The pedestrian zone also should be clear of street trees, tree grates, and other landscaping.
 - d. Second-story terraces for outdoor dining are encouraged where sidewalk space is limited. Second-story terraces shall be integrated into the design of restaurants and the overall building.
 - e. A temporary or seasonal barrier or edge is encouraged to define outdoor café spaces and ensure the pedestrian clear zone. The barrier should be a simple decorative railing, fence, planters, or similar element. The design of the barrier should reflect the style of the building and coordinate with the downtown streetscape, and shall be reviewed and approved by the village's designated hearing body.
- (11) *Parking.* Refer to subsection (c), Regulating framework, and the Municipal Code and Parking Lot Landscaping and Design Guidelines for additional information on parking standards. In addition, the following requirements will apply:
- a. *Design.*
 - 1. Off-street parking spaces may be open or enclosed in a building.
 - 2. Parking areas shall be screened with fencing and landscaping at street and sidewalk edges and adjacent to residential buildings. Fencing, walls, and landscaping shall be a maximum of 30 to 42 inches in height and shall preserve sight lines at entrances/exits to reduce conflicts between pedestrians and motorists.
 - 3. All exposed/visible walls on freestanding parking structures, as well as on parking structures in buildings, should be screened and articulated with architectural treatment.
 - 4. Designated parking spaces, except those within townhomes/rowhomes, shall be marked with permanent marking materials and maintained in a clearly visible condition.
 - 5. Parking area lighting shall be designed to Village standards and minimize impact on surrounding properties.
 - 6. Parking areas shall be designed to accommodate snow removal, such as by eliminating unnecessary obstacles and providing for temporary snow storage where feasible.
 - 7. Off-street parking spaces shall have a vertical clearance of at least seven feet if provided within a building.
 - b. *Location.*
 - 1. Parking areas in front of buildings are prohibited unless otherwise specified in the Code.
 - 2. Mid-block parking lots should be limited to avoid breaking up the shopping streetwall.
 - 3. Business owners, employees, and residents should park in the rear of parking lots or on the upper floors of parking structures rather than occupy spaces on streets or prime parking locations for shoppers, visitors, and restaurant patrons.
 - 4. Bicycle parking shall be considered and placed in locations that are safe and convenient to high-traffic destinations in such a manner as to not impede pedestrian circulation.

c. *Sharing.*

1. Adjacent commercial parking lots must be connected, where feasible, if access to the two lots is not available via an alley, to facilitate trips between commercial uses.
2. If a driveway is needed to access rear parking lots, it must be shared with future development on adjacent properties via easements if the future development cannot be accessed from the alley or side street.

Figure e-16: Parking Layout



(12) *Streetscape/landscape.* An attractive and effective streetscape will provide visual continuity from block to block and continue to define downtown as a special place. Developers should incorporate the Village's streetscape design into development, including standards for street trees, street furniture, pavers, and other streetscape elements. This will help visually unify the district.

a. *Design.*

1. The current streetscape design installed along Glenview Road should be enhanced and continued throughout downtown, with modifications to match different road and sidewalk widths.
2. Landscape and hardscape for large corner bump-outs should be treated on an individual basis because of particular needs and situations in each area. Individual elements should follow the overall downtown streetscape design theme.
3. The developer is responsible for installing Village-approved streetscape elements within all new developments.
4. Developments are encouraged, where feasible, to include "greening" elements such as planter boxes, gardens on terraces or patios, landscaping, and other environmentally attractive and beneficial features.

b. *Plazas and open spaces.*

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1. Plazas and open spaces shall be considered within new developments where feasible.
 2. Plazas and pocket parks shall be adjacent to a public ROW and be open to the public at all times. Edges of a plaza or pocket park internal to the site (not adjacent to the ROW) should be defined with active use on the ground floor. If a plaza or pocket park is adjacent to surface parking, landscape screening and/or fencing should be used to buffer views into parking.
 3. Plazas and pocket parks should include benches, trash receptacles, and bicycle racks consistent with Glenview Road streetscape standards.
 4. Plazas and pocket parks shall be no smaller than 900 square feet and will not be calculated as part of a building's building line requirements (see subsection (d), Street frontages), and are required for all developments over one and one-half acres.
 5. Decorative paving such as brick, clay pavers, stone, decorative pre-cast concrete pavers, or stamped concrete shall be considered when designing the hardscape for new plazas, open spaces, and bump-outs.
 6. Existing and future open spaces should incorporate special features such as fountains, artwork, plantings, and other elements.
 7. Where pedestrian paths or pass-throughs are used to access parking, they shall incorporate decorative fencing, arches, lighting, paving, and signage. When pass-throughs are located between buildings, outdoor dining areas (where applicable), active storefronts (where applicable), overhead low-voltage cable lighting is encouraged. Pass-throughs between buildings shall be a minimum of 15 feet wide.
 8. Plaza and pocket parks can be used to access underground storm water detention for maintenance.
- c. *Street furniture.*
1. Decorative metal benches, trash receptacles, and bike racks shall be provided at high-activity pedestrian areas including pocket parks and plazas.
 2. Decorative planters shall be placed in pocket parks, plazas, and along pedestrian paths and sidewalks where they will not impede safe flow of pedestrians.
- d. *Food trucks.*
1. Food trucks are a permitted use in downtown with a valid permit or license approved by the Village. Additionally, food trucks are allowed to operate on Glenview Road within the ROW in designated areas, with approval by the Village or in designated areas within parks, if the Village chooses to create such areas. Food trucks may operate in surface parking lots that are visible from a public ROW at the discretion of the owner. It is encouraged that food trucks be located near high traffic areas and during downtown festivals or events.
 2. Food trucks are not allowed to park overnight without prior approval by the Village and only during multi-day events in downtown.
 3. Food trucks shall maintain a five-foot pedestrian clear zone on the sidewalk and not interrupt or encroach on traffic lanes or transit.
- e. *Art displays.*
1. Art displays installed on private property is encouraged to be added with new development. Art displays can be permanent or temporary displays and includes, but is not

limited to sculpture, murals, or interactive elements as is appropriate for outdoor application.

2. Art displays shall not block the five-foot pedestrian clear zone on sidewalks or automobile traffic and is subject to approval by the Village.
3. The art display will be installed and maintained at the owner's expense.
4. The property owner is responsible for adequate property insurance for the artwork.
5. Art displays cannot include the name of commercial businesses. Trademarked logos, or explicit content.

f. *Parklets.*

1. A parklet is a small seating area or green space located alongside a sidewalk, typically located in on-street parking spaces. Parklets are encouraged to be located adjacent to high traffic areas and near shops and restaurants.
2. A parklet cannot take up more than two parking spaces or 20 linear feet.
3. Parklets are allowed on public property in downtown with a valid permit approved by the Village, insurance, and in compliance with all other applicable requirements.
4. If a parklet is located on private property, the owner must show that any parking spaces removed for the parklet have been replaced on the adjacent property with the equivalent spaces that are available for public use at all times, free, and accessible. Temporary or seasonal parklets are exempt from this requirement with a valid permit approved by the Village.

g. *Pop-up retail.*

1. Pop-up or temporary retail is allowed in parklets, vacant storefronts, or in designated areas within parks (during festivals and events only), if the Village chooses to create such areas, subject to approval by the Village.
2. Pop-up retailers are allowed to utilize temporary shelters and shading devices as long as they do not block the five-foot pedestrian clear zone, encroach on drive lanes, or create visual barriers to drivers.
3. Pop-up retail cannot keep or store shade structures, tables, or other equipment overnight without approval by the village.
4. Retailer goods and services shall comply with the requirements of chapters 6, 22, 38, and 46 of the Municipal Code.

- (13) *Pattern book.* The pattern book is meant to highlight a range of architectural styles, details, and intentions that reflect the preferred look and feel of new development in Downtown Glenview. These images are reflective of multiple community visual preference exercises and other means of garnering community input over an 18-month process. The pattern book is not meant to be a definitive or exhaustive review of design styles or details, rather it is intended as a guide for future development direction.

The pattern book categories include:

- Architectural style
- Façades
- Recesses/projections

-
- Overhangs/awnings
 - Entryways
 - Tower elements
 - Roofs
 - Materials
 - Windows/fenestration
 - Expression of base, middle, and top
 - Architectural details

Architectural style.

Intent: While no specific architectural style(s) have been chosen for downtown, building designs should complement the historic nature of the area, support a look of being "built over time", and reflect an expression of high-quality, timeless materials and design details.



Façades.

Intent: Façades play a very important role in creating the "character" of downtown. Façades evoke immediate impressions about the quality of the development. Articulation of façades can provide visual interest to the building and highlight important areas such as entries, display windows, and outdoor dining while positively impacting the visual scale of buildings.



Recesses/projections.

Intent: Recesses or projections in the building facade provide an opportunity to reduce the scale of long building façades, articulate important architectural features, and provide visual interest to the buildings. Highlighting areas such as entries, important displays, outdoor gathering/dining areas, or structural repetition with recesses or projections help to organize and unify buildings within downtown.



Overhangs/awnings.

Intent: Overhangs and awnings can be used to emphasize building entries, provide protection from rain, create signage opportunities for tenants, and reduce the visual mass of buildings at the sidewalk. Awnings can be structural or fabric.



Entryways.

Intent: Entryways should be recessed to provide visual interest to the ground plane, rain protection, and to reduce conflicts of doors opening directly onto the sidewalk. Entries can be combined with awnings/canopies, signage, display windows and/or plantings to creating a welcoming entrance into a building or shopfront. If a building includes multiple entries, visual consistency between the entries is important to create a unified streetscape.



Tower elements.

Intent: Tower elements can be used to add visual interest to a building and mark important intersections or gateways in downtown. As shown in the images below, tower elements need not project higher than the primary roof height to be effective.



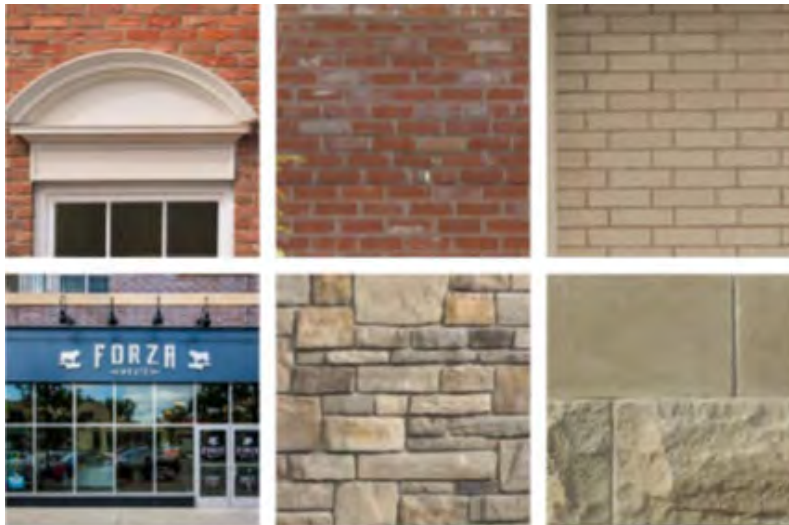
Roofs.

Intent: Roofs create visual interest in buildings, and screen roof top mechanical equipment. Used properly, roofs can create a sense of rhythm and hierarchy in buildings with long façades. Variations in roof forms can be used to mark building entries, or frame important gateways in downtown.



Materials.

Intent: Building materials should be muted, natural, and evoke high-quality, timeless architecture. Brick, stone, and/or other masonry products should be considered for primary building materials, while wood, metal or stucco should be used sparingly as accent materials.



Windows/fenestrations.

Intent: Windows should be organized on the building facade to best serve the intended use of the buildings and to also provide structure, rhythm, and repetition to the building facade. In ground floor retail applications, windows (along with entrances) are the most visible aspect of the building and should be designed to provide a harmonious "base" to a building.



Expression of base, middle, and top.

Intent: All multi-story buildings should express the base, middle, and top through step-backs, architectural details, and/or material changes to form a unified building facade that include proportionality that is consistent throughout downtown.



Architectural details.

Intent: Architectural details such as brick or masonry details, inset windows, metal work, etc. provide visual interest to buildings and support the "built over time" look and feel that is appropriate for downtown.



(f) *Administration.*

- (1) *Project review.* All redevelopment projects within the district are subject to the regulatory review process as described below.
- (2) *Site plan review.* All redevelopment projects within the district shall be subject to site plan review approval as set forth in chapter 54, article IV, of the Municipal Code.
- (3) *Appearance review.* All redevelopment projects within the district shall be subject to the Appearance Code as set forth in chapter 54, article III, and the design standards in section 98-135(e) of this chapter of the Municipal Code.
- (4) *Conditional uses.* Land uses specified as conditional uses by section 98-84 shall be heard by the designated hearing body and the board of trustees pursuant to chapter 98, section 98-43(c)(2), subject to the standards set forth in section 98-50 of this chapter.
- (5) *Variations.* Variations from the regulations of this Downtown Development Code, except for section 98-135(e), Design standards, shall be considered by the designated hearing body as set forth in chapter 98, section 98-47, of the Municipal Code. Exceptions from the regulations of section 98-135(e) shall be considered by the designated Hearing Body as set forth in chapter 54, article III, of the Municipal Code.
 - a. *Authorized variations.* Variations from the regulations of this Downtown Development Code shall be recommended by the board only in accordance with the standards set forth in chapter 98, section 98-47(c), and may be granted only to vary any of the regulations relating to:
 1. *Regulating framework, section 98-135(c).*
 - i. Use.
 - ii. Height.
 - iii. Building location.
 - iv. Parking.
 2. *Street frontages, section 98-135(d).*
 - i. Street and sidewalk standards.
 - ii. Building setbacks.
 - iii. Building step-backs.

-
- iv. Parking placement.
 - v. Access.
- (6) *Non-conformities.* Except as otherwise provided by the provisions of this chapter, the provisions of chapter 98, article V, section 98-251 et seq., Non-conformities, shall apply to existing land uses and structures.
- (7) *Planned developments.* Separately from the requirements of this chapter, redevelopment projects can be proposed in accordance with the provisions of chapter 98, article X, Planned Developments. Redevelopments pursued in accordance with a planned development shall generally correspond to the requirements of the underlying zoning district.
- (g) *Definitions.* The following words, terms and phrases, when used in this Downtown Development Code, shall have the meanings ascribed to them in chapter 98, section 98-4, or this section, except where the context clearly indicates a different meaning.

Access drive. Dedicated public right-of-way, other than a street, that affords access from a street from the front of a lot.

Alley. Dedicated public right-of-way, other than a street, that affords a secondary means of access to abutting property, generally at the rear of the lots.

Apartment. Multi-family residential development with for-rent units.

Awning. A metal tubular structure over a storefront or entrance that provides a framework for a canvas covering and that provides protection from sun and inclement weather for patrons walking below.

Balcony. Platform that is cantilevered from a building wall and is enclosed by a parapet or railing.

Bay/box windows. Generally, a U-shaped enclosure that extends the building's interior space beyond the plane of the exterior wall and/or the building line.

Building line. A line/plane indicated on the regulating plan defining the street frontage which extends vertically and at which the building façade shall be placed. The required building line for each site is shown in the street frontage section.

Bump-out. A curb improvement common at intersections or adjacent to on-street parking stalls used in lieu of pavement striping to provide additional vehicle protection, pedestrian refuge area, or landscape beds.

Commercial. Retail, service, restaurant, and office uses.

Condominium. Multi-family residential development with for-sale units.

Development Code. The Downtown Development Code.

Development project. Any use in the Downtown Development District that results in the construction of a new building or structure, major modifications to the interior dimensions of an existing building or structure (including building additions), alteration in area to a paved parking lot or the addition of or change to a curb cut.

District. The Downtown Development District.

Downtown Development District. Includes:

- Glenview Road from Washington Street to one block east of Waukegan Road;
- Waukegan Road from Lake Avenue to Henley Street;
- Railroad Avenue from Dewes Street to just north of Grove Street;
- Dewes Street from Washington Street to Waukegan Road;

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- Portions of Pine, Grove, Church, Prairie, Harlem, McLean, and Maplewood that are side streets for the major roads listed above;
 - All as designated on the zoning map, unless otherwise expressly exempted or provided in this Downtown Development Code.

Downtown Revitalization Plan. The plan approved by the village board of trustees in November 2006, which serves as the guiding framework for this Downtown Development Code.

Eave. Edge of a roof that projects beyond the vertical building facade.

Floor. The space within a building and above grade that is between the midpoint of the floorplate and the midpoint of the floorplate for the floor above. Of, if there is no floor above, the ceiling or roof above. See also Ground floor.

Floor area. The area, measured at floor level, contained between the exterior wall surfaces of a building or other structure or, in the case of rooms, between the exterior wall surface of the building and the center of the enclosing partitions, or between the centerlines of partitions where no exterior wall is affected. For the purpose of determining off-street parking and loading requirements, the term "floor area" shall be defined to be the sum of the gross horizontal areas of the several floors of a building, or portion thereof, devoted to a use requiring off-street parking or loading as provided in this article. Such area shall include accessory storage areas located within selling or working space devoted to retailing activities, the production or processing of goods, or business or professional offices. However, the term "floor area" shall not include floor space devoted primarily to the housing of mechanical or electrical equipment, or storage purposes, except as otherwise noted in the Municipal Code, nor floor space and ramps, aisles and maneuvering space devoted to off-street parking or loading facilities, nor basement floor space, other than the area devoted to merchandising activities, the production or processing of goods, business or professional offices, or dwelling uses.

Front yard. A yard extending along the full length of a front lot line and back to a line drawn parallel to the front lot line at a distance there from equal to the depth of the required front yard. When a lot fronts onto two non-intersecting streets (through lot), the front yard shall be determined to be on the street to which all of the houses along that block are oriented, and the opposite yard shall be treated as a rear yard.

Ground floor. The first level of a building at or above grade. The floor height of the ground floor is measured from the grade to the midpoint of the floorplate of the floor above (or, if there is no floor above, the ceiling or roof above.)

Institutional. A building that provides municipal services and is owned by the village. (Examples include the Village Hall, fire stations, and police departments.)

Interior side yard. A yard extending along a side lot line that is not adjacent to a street and back to a line drawn parallel to the side lot line at a distance there from equal to the width of the required minimum side yard, but excluding any area encompassed within a front yard or rear yard. Dimensions of side yards specified in the district regulations of this chapter refer to the required width of each side yard, rather than to the total width of both side yards, unless otherwise specified.

Mixed-use. A building that includes two or more complementary uses, such as residential, retail, office, and service.

Multi-family building. Residential development with dwelling units that share common walls with units that are laterally and vertically adjacent.

Office use. Establishments engaged in the provision of goods and services in an office setting, usually by professionals. This includes business, government, professional, medical, or financial services and includes such occupations as accountants, economic consultants, doctors, dentists, architects, software developers, researchers, and designers.

Parapet. A low protective wall or barrier at the edge of a balcony or roof.

Porch. A covered but otherwise open platform that provides a transition between the interior of a building and the public space of the street.

Primary street. The street that abuts the property and includes the building's primary entrance and front facade. See also side street.

Rear yard. A yard extending along the full length of the rear lot line, except as qualified in the definition in this section of the term yard, side, and back to a line drawn parallel to the rear lot line at a distance there from equal to the depth of the required rear yard.

Regulating framework. Establishes the desired physical form for the downtown development district and sets development parameters such as permitted land uses, street types, building height, siting, and setbacks.

Residential. A building or part of a building containing dwelling units.

Retail use. Businesses that provide goods/ merchandise to the general public for personal or household consumption and render services incidental to the sale of those goods. Also, restaurants.

Service use. Businesses that provide services (as opposed to goods) to the general public. (Examples include hotels, finance, real estate, insurance, travel agencies, health and educational services, and galleries.)

Setback. The distance between a reference line, which shall be a public or private right-of-way or easement, whichever shall be applicable, and a building, or portion thereof, on the lot in question. In the case of a lot adjoining a public or private street right-of-way, the setback shall be the difference between such right-of-way line and the building closest to the right-of-way on the lot. In the case of an unrecorded easement, the director of community development shall determine the setback in a manner which will provide a reasonably consistent setback.

Side street. For buildings on corner lots, the street that abuts the property but does not include the building's primary entrance (except those with corner entrances) and front facade. See also primary street.

Side yard, corner lot of side street. A yard extending along a side lot line that is adjacent to a street and back to a line drawn parallel to the side lot line at a distance there from equal to the width of the required minimum side yard, but excluding any area encompassed within a front yard or rear yard. Dimensions of side yards specified in the district regulations of this chapter refer to the required width of each side yard, rather than to the total width of both side yards, unless otherwise specified.

Step-back. The minimum distance a building's upper-floor facade must be placed from the facade of the floor or floors below.

Stoop. A staircase on the facade of a building that leads either to a small, unwallled entrance platform or directly to the main door.

Townhouse/rowhouse. Single-family attached homes that share walls with units laterally adjacent to them.
(Ord. No. 6466, § 4, 1-18-2022)